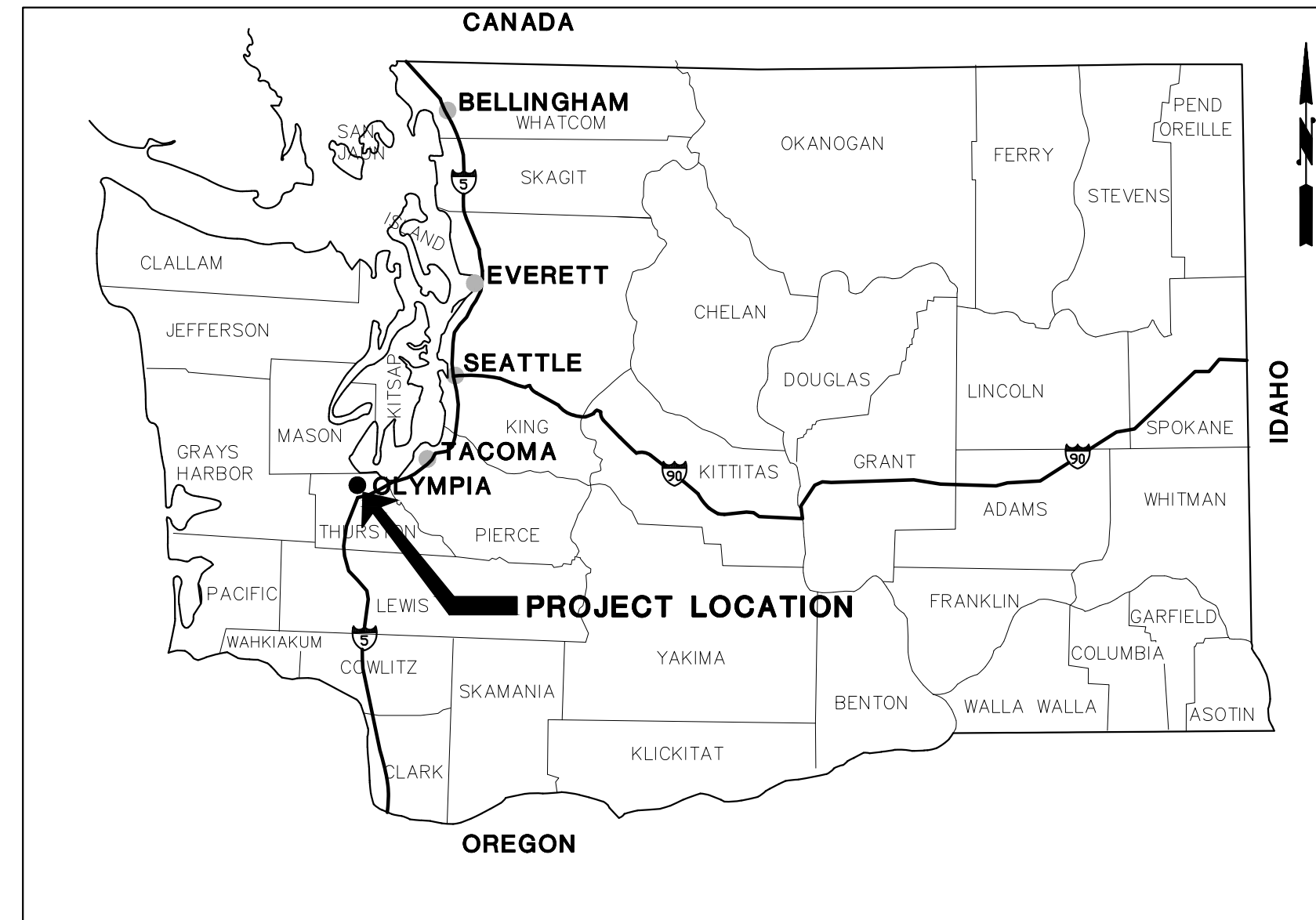
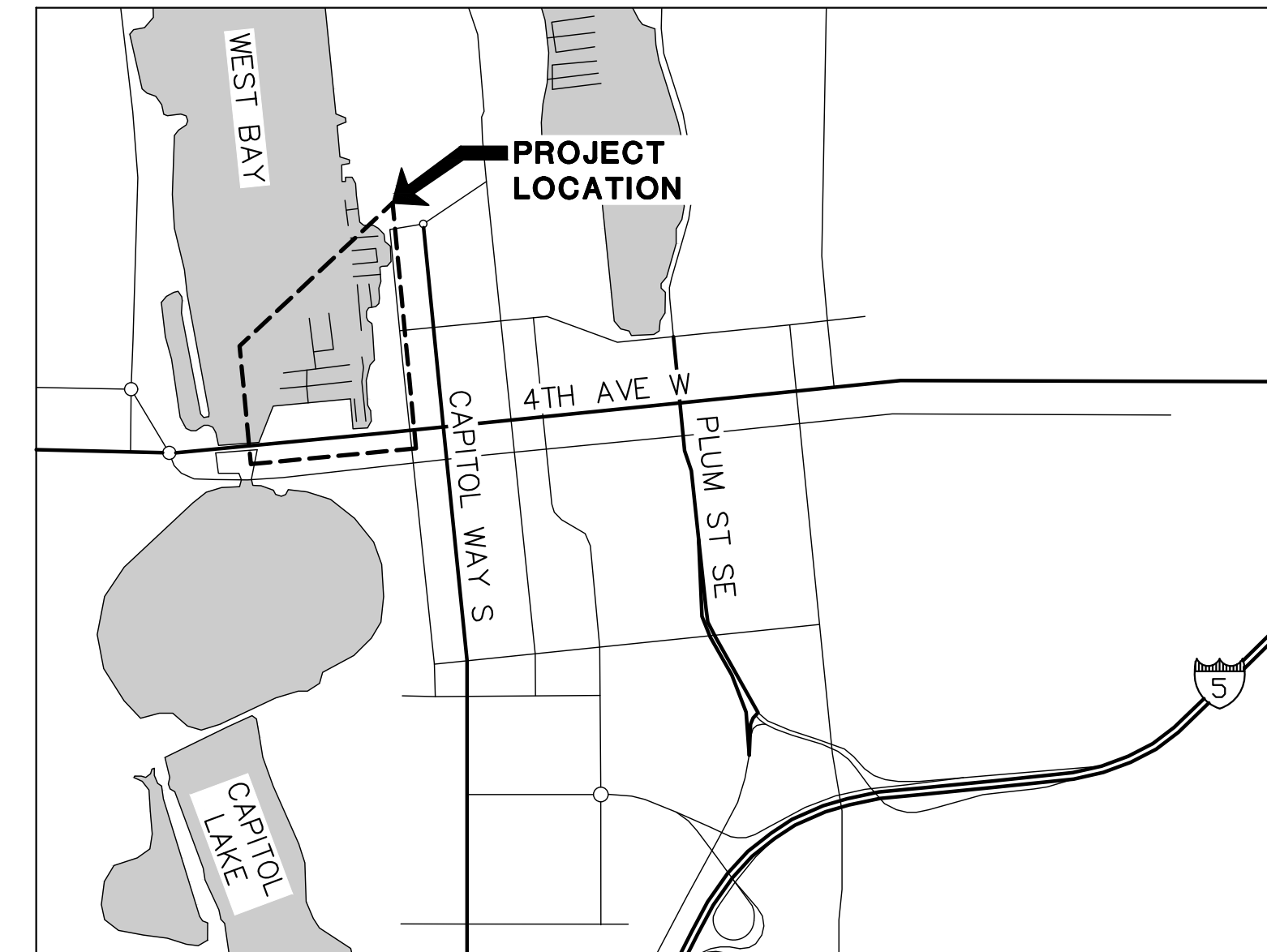


PERCIVAL LANDING 2021 REPAIRS

CITY OF OLYMPIA PARKS, ARTS, AND RECREATION DEPARTMENT
 OLYMPIA, WASHINGTON
 PROJECT NO. 1956H



AREA MAP
NOT TO SCALE



VICINITY MAP
NOT TO SCALE

Olympia City Council

Cheryl Selby, Mayor

Yên Huỳnh

Dani Madrone

Clark Gilman

Lisa Parshley

Renata Rollins

Jim Cooper

Jay Burney, City Manager

Project Team

Jake Lund, Project Manager

Paul Simmons, Director of Parks, Arts and Recreation

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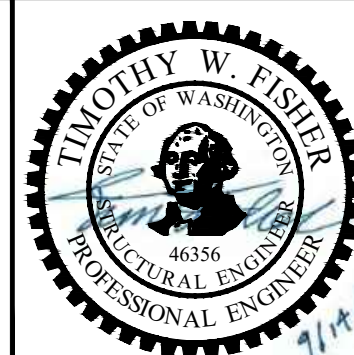
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 APPROVED BY: JRG
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**PERCIVAL LANDING
 2021 REPAIRS**

COVER SHEET & DRAWING INDEX

FOR BID

ONE INCH
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G-1

SHEET NO. 1 OF 21

GENERAL NOTES

CODES AND REFERENCE STANDARDS

ALL MATERIALS, WORK AND TESTING SHALL CONFORM TO THE FOLLOWING CODES AND REFERENCE STANDARDS IN ADDITION TO THOSE STATED ELSEWHERE IN THE CONTRACT.

- 2018 IBC AS AMENDED BY THE CITY OF OLYMPIA
- 2018 IRC AS AMENDED BY THE CITY OF OLYMPIA
- ACI 301-10: SPECIFICATIONS FOR STRUCTURAL CONCRETE
- ACI 305R-10: HOT WEATHER CONCRETING
- ACI 306R-10: COLD WEATHER CONCRETING
- ACI 308R-01: GUIDE TO EXTERNAL CURING OF CONCRETE
- ACI 318-14: BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE AND COMMENTARY
- AITC TIMBER CONSTRUCTION MANUAL
- WCLIB STANDARD GRADING RULES FOR WEST COAST LUMBER - NO. 16
- AWPA WOOD PRESERVATION STANDARDS
- WWPI BEST MANAGEMENT PRACTICES FOR THE USE OF TREATED WOOD IN AQUATIC AND OTHER SENSITIVE ENVIRONMENTS (1 AUGUST 2006)
- ASTM, CURRENT EDITION
- 2018 NDS

CONSTRUCTION LOADS

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR CALCULATIONS AND DETERMINATIONS OF STRUCTURE CAPACITY FOR THE ANTICIPATED LOADS OF THE CONSTRUCTION EQUIPMENT TO BE USED, AND THE STABILITY OF THE STRUCTURE UNTIL IT IS COMPLETE.

GENERAL

- THE CONTRACTOR SHALL VERIFY INFORMATION SHOWN ON THE DRAWINGS, OTHER CONTRACT DOCUMENTS AND BRING ANY CONFLICTS TO THE ATTENTION OF THE ENGINEER BEFORE BEGINNING AFFECTED WORK. THE ENGINEER WILL RESOLVE ANY SUCH CONFLICTS.
- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- PROVIDE TEMPORARY SUPPORT TO THE EXISTING STRUCTURE AS REQUIRED TO MAINTAIN STABILITY, AVOID UNDUE STRESS, AND PREVENT DAMAGE PRIOR TO AND DURING REPAIRS.
- THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH ALL PERMITS, LAWS AND INDUSTRY STANDARDS.
- ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS TO EXISTING FEATURES, STRUCTURES, AND UTILITIES THAT ARE TO REMAIN, SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PREVENT, AVOID, AND MINIMIZE THE POTENTIAL FOR DEMOLITION, CONSTRUCTION OR OTHER DEBRIS TO FALL INTO THE WATER BY UTILIZING TARPS, FLOATING PLATFORMS OR OTHER MEANS. IN THE EVENT THAT DEBRIS ENTERS THE WATER, THE CONTRACTOR SHALL IMMEDIATELY REMOVE THE DEBRIS AND DISPOSE OF IT IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS. THE CONTRACTOR SHALL MONITOR OVERWATER DEMOLITION AND CONSTRUCTION ACTIVITIES TO FACILITATE IMMEDIATE REMOVAL OF ANY DEBRIS THAT ENTERS THE WATER. CONTAINMENT METHODS SHALL BE MODIFIED IF MONITORING INDICATES THAT THE METHODS USED ARE NOT EFFECTIVE.
- ALL MATERIAL SHALL BE NEW AND UNDAMAGED, UNO.
- ALL REPAIR AND REPLACEMENT WORK SHALL BE LIKE-KIND USING NEW MATERIAL, UNO.
- CONTRACTOR SHALL HAVE AT LEAST ONE EMPLOYEE ONSITE SUPERVISING WORK THAT HAS AT LEAST 7 YEARS OF EXPERIENCE WITH THE PARTICULAR TRADE SKILL BEING PERFORMED AT ANY GIVEN TIME.
- UNUSED AND EXCESS MATERIAL SHALL BECOME PROPERTY OF CONTRACTOR AND REMOVED FROM SITE.
- ANY EXISTING STRUCTURES OR ASSEMBLIES THAT ARE TO REMAIN, BUT REQUIRE TEMPORARY DISASSEMBLY, MAY BE REASSEMBLED W/ SAME MEMBER AND HARDWARE, UNO, PROVIDED THE HARDWARE AND/OR MEMBER ARE IN LIKE-NEW AND UNDAMAGED CONDITION, AS DETERMINED BY THE ENGINEER OTHERWISE THIS HARDWARE AND/OR MEMBER SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

PERMITS

ALL WORK SHALL CONFORM TO THE ENVIRONMENTAL PERMIT REQUIREMENTS IN THE CONTRACT. A BUILDING PERMIT IS NOT REQUIRED FOR THE WORK SHOWN IN THE CONTRACT.

ELEVATIONS

- ELEVATIONS AND CONTOURS ARE IN FEET AND INDICATE DISTANCE ABOVE OR BELOW TIDAL DATUM MLLW = 0.0.
- CONTOURS AND BATHYMETRY SHALL BE CONSIDERED APPROXIMATE.

TIDES

TIDAL ELEVATIONS (REFERENCED TO TIDAL DATUM MLLW = 0.0) ARE BASED ON NOAA STATION 9446969.

- | | |
|----------------------------------|--------|
| a. HIGHEST OBSERVED TIDE | 17.94' |
| b. MEAN HIGHER HIGH WATER (MHHW) | 14.56' |
| c. MEAN HIGH WATER (MHW) | 13.55' |
| d. MEAN LOW WATER (MLW) | 3.07' |
| e. MEAN LOWER LOW WATER (MLLW) | 0.00 |
| f. LOWEST OBSERVED TIDE | -4.33' |

PUBLIC USE DURING CONSTRUCTION

IT IS EXPECTED THAT PERCIVAL LANDING WILL REMAIN OPEN TO THE PUBLIC DURING CONSTRUCTION. CONTRACTOR SHALL SUBMIT ANY TRAFFIC OR PEDESTRIAN CONTROL PLANS TO THE CITY FOR REVIEW AND APPROVAL PRIOR TO THE CLOSURE. CONTRACTOR MAY CLOSE OFF AREAS OF PERCIVAL LANDING AS REQUIRED TO ACCOMPLISH THE PROJECT. WHERE THE CONTRACTOR NEEDS TO CLOSE OFF LARGE AREAS OF PERCIVAL LANDING DURING CONSTRUCTION, WHERE PRACTICAL THE CONTRACTOR SHALL PROVIDE A PEDESTRIAN ROUTE AROUND THE CLOSED OFF AREAS. PERCIVAL LANDING IS A PUBLIC SPACE AND ALL APPROPRIATE SAFETY PRECAUTIONS SHALL BE TAKEN BY THE CONTRACTOR.

CONSTRUCTION SCHEDULE

CONTRACTOR SHALL PROVIDE THE CITY A CONSTRUCTION SCHEDULE. THE CONSTRUCTION SCHEDULE SHALL SPAN FROM AWARD TO DEMOBILIZATION AND SHALL BE UPDATED AND RESUBMITTED IF IT BECOMES OUTDATED. THE SCHEDULE SHALL BE SPECIFIC ENOUGH THAT THE CITY CAN KNOW WHAT ACTIVITIES, NAMED IN THE CONTRACT, WILL BE PERFORMED AND WHEN. CONTRACTOR SHALL COORDINATE AND COMMUNICATE ACTIVITIES REGULARLY, SO THE CITY CAN PLAN AROUND WORK.

SUBMITTALS

EACH SUBMITTAL SHALL BE APPROVED BY THE ENGINEER PRIOR TO STARTING THE ASSOCIATED WORK OR ORDERING THE ASSOCIATED MATERIAL. THE FOLLOWING ARE REQUIRED SUBMITTALS FOR THIS PROJECT, IN ADDITION TO ANY OTHER SUBMITTALS REQUIRED WITHIN THE CONTRACT DOCUMENTS. CONTRACTOR SHALL BE RESPONSIBLE FOR MATERIALS AND METHODS THAT MEET PROJECT REQUIREMENTS, PROVIDE GOOD FUNCTION AND LONGEVITY. APPROVAL OF SUBMITTALS DOES NOT ABSOLVE CONTRACTOR OF THIS RESPONSIBILITY. SUBMITTALS 1 THROUGH 4, BELOW, ARE DUE AT THE PRE CONSTRUCTION CONFERENCE AND SHALL BE APPROVED BEFORE STARTING ANY REPAIR WORK.

- LIST OF SUBMITTALS
- CONSTRUCTION SCHEDULE
- WORK PLAN
- TRAFFIC AND PEDESTRIAN CONTROL PLANS
- MATERIAL CERTIFICATIONS, MILL CERTIFICATIONS AND MATERIAL CUT SHEETS
- SHOP DRAWINGS
- PRE-CONSTRUCTION D AND E FLOAT EXISTING SUPPLEMENTAL FLOTATION AS-BUILT SKETCHES
- GATE SELF-CLOSING SPRING ATTACHMENT DETAIL SKETCHES, 3 LOCATIONS
- FINAL AS-BUILTS
- ZINC RICH PAINT COLOR SWATCH, PLASTIC LUMBER SAMPLE FOR COLOR
- D AND E FLOATS BUOYANCY CALCULATIONS

WOOD/TIMBER

- ALL WOOD/TIMBER SHALL BE PACIFIC COAST DOUGLAS FIR, GRADE NO. 1, UNO, AND SHALL CONFORM TO THE LATEST SPECIFICATIONS OF THE WCLIB.
- HOLES FOR BOLTS SHALL BE 1/16 INCH LARGER THAN THE BOLT DIAMETER. LEAD HOLES FOR LAG SCREWS SHALL BE PRE-BORED TO 65 PERCENT OF THE SHANK DIAMETER.
- INSTALL FASTENERS IN A MANNER THAT PREVENTS SPLITTING OF WOOD, AS APPROVED BY THE ENGINEER WHICH MAY INCLUDE USE OF PILOT/LEAD HOLES, ETC.
- ALL SHOP AND FIELD CUTS, BORED HOLES AND INJURIES OF TIMBER SHALL BE TREATED IN ACCORDANCE WITH AWPA STANDARD M4. FIELD TREATMENT SHALL BE COPPER NAPHTHENATE.
- BOARDWALK TIMBER DECKING BOARDS SHALL BE UNTREATED.
- ALL WOOD/TIMBER SHALL BE PRESSURE TREATED IN ACCORDANCE TO AWPA STANDARDS T1, U1 AND UC4B WITH A MINIMUM RETENTION OF 0.60 POUNDS PER CUBIC FOOT OF AMMONIACAL COPPER ZINC ARSENATE (ACZA), UNO. BEST MANAGEMENT PRACTICES (BMP'S) SHALL BE INCORPORATED AS SET FORTH BY THE WESTERN WOOD PRESERVERS INSTITUTE (WWPI). SEE DWG S-11 FOR BENCH MEMBERS THAT REQUIRE A DIFFERENT TREATMENT PROCESS.
- ALL TIMBER SHALL BE INSOFAR AS PRACTICABLE CUT TO LENGTH AND BORED BEFORE TREATMENT.
- PLYWOOD SHALL BE UNTREATED MARINE GRADE PLYWOOD CONFORMING TO PS-1, EXTERIOR.
- ALL WOOD/TIMBER SHALL BE STRAIGHT, SQUARE, TRUE, LEVEL, PLUMB, PLANE AND SHALL BE CUT AND BORED THE SAME. WOOD/TIMBER SHALL BE CUT TO FIT TIGHT WHERE INSTALLED. ALL CUTS SHALL BE SMOOTH AND FREE OF ABRASION AND BURRS FROM POOR WORKMANSHIP. MATING SURFACES SHALL BE CUT TO FULLY CONTACT. WARPED, BOWING OR TWISTED WOOD/TIMBER ARE NOT ACCEPTABLE AND SHALL NOT BE USED. ANY WARPED, TWISTED OR POORLY FITTING WOOD INSTALLED BY THE CONTRACTOR SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE, AS DETERMINED BY THE ENGINEER. BORED HOLES SHALL BE ROUND.

REINFORCED CONCRETE

- REINFORCING STEEL SHALL BE DEFORMED STEEL BARS CONFORMING TO ASTM A 615, GRADE 60.
- CAST-IN-PLACE CONCRETE SHALL HAVE A 5000 PSI MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS.
- CONSTRUCTION JOINTS SHALL BE PROVIDED ONLY AS NOTED ON THE DRAWINGS AND AS APPROVED BY THE ENGINEER.
- SPILLING FRESH CONCRETE INTO THE WATER DURING CONCRETE PLACEMENT ACTIVITIES IS STRICTLY PROHIBITED.
- CONCRETE SHALL BE HIGH STRENGTH CONCRETE BY SAKRETE OR EQUIVALENT.
- ENGINEER SHALL BE GIVEN 2 DAYS NOTICE PRIOR TO PLACING CONCRETE.

DEMOLITION

ALL DEMOLITION MATERIAL, EXCEPT AS NOTED, SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE COMPLETELY REMOVED AND DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE W/ ALL LAWS AND PERMITS. THE REMOVAL, HANDLING AND DISPOSAL OF ALL DEMOLITION MATERIALS, INCLUDING CREOSOTE-TREATED TIMBERS SHALL BE IN STRICT ACCORDANCE WITH ALL STATE AND FEDERAL REQUIREMENTS. PROPER DISPOSAL OF ALL DEMOLITION AND CONSTRUCTION MATERIALS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE ITSELF WITH THE MATERIALS TO BE DISPOSED OF AND ALL GOVERNING AGENCIES AND PERMIT REQUIREMENTS.

SHIM BOARDWALK TIMBER DECKING

- UP TO 295 LF OF BOARDWALK 12" NOMINAL WIDTH DECKING BOARDS SHALL BE SHIMMED, WHERE IDENTIFIED BY THE CITY FOLLOWING AWARD.
- REMOVE, SHIM AND REINSTALL BOARDWALK TIMBER DECKING BOARDS.
- REMOVED DECKING HARDWARE THAT IS BENT OR DAMAGED SHALL BE REPLACED.
- CARE SHALL BE TAKEN TO NOT DAMAGE THE EXISTING DECKING BOARDS THAT ARE TO BE REINSTALLED.
- SHIM W/ PLYWOOD TO MAKE TIMBER DECKING BOARDS EVEN W/ ADJACENT DECKING BOARDS AND CONCRETE. A SINGLE PIECE OF PLYWOOD SHALL BE USED FOR FULL SHIM THICKNESS WHERE POSSIBLE.
- CHASE EXISTING LAG SCREW THREAD PROFILE INTO EXISTING SUBSTRATE WHEN REINSTALLING DECKING BOARDS.
- IF AN EXISTING LAG SCREW HOLE PROFILE BECOMES STRIPPED, THEN ADD A 6" HEADLOK SCREW ADJACENT.
- PLYWOOD SHIMS SHALL BE SIZED FOR FULL BEARING AREA OF DECKING BOARD.

REPLACE BOARDWALK TIMBER DECKING BOARDS

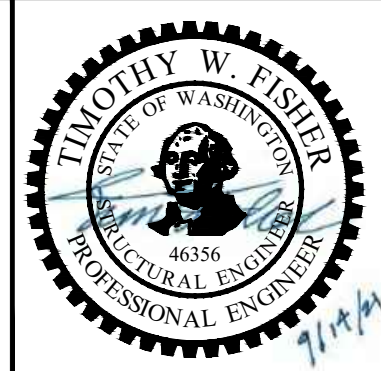
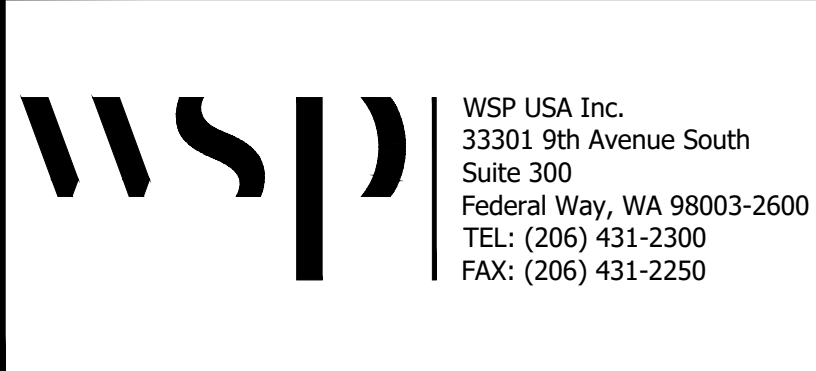
- UP TO 240 LF OF BOARDWALK 12" NOMINAL WIDTH DECKING BOARDS SHALL BE REPLACED, WHERE IDENTIFIED BY THE CITY FOLLOWING AWARD.
- SHIM REPLACEMENT DECK BOARDS AS NEEDED TO PROVIDE AN EVEN SURFACE W/ SURROUNDING WOOD AND CONCRETE SURFACES.
- DECKING BOARDS REPLACED AS PART OF THE 240 LF PROJECT TOTAL THAT ARE ALSO SHIMMED, SHALL NOT ALSO COUNT TOWARDS THE 295 LF PROJECT TOTAL FOR SHIMMING.
- EXISTING END BUTTED BOARDS TO REMAIN THAT REQUIRE BEING CUT SHALL BE SECURED OVER EACH SUPPORT AND MAY REQUIRE ADDITIONAL SPIKES OR LAGS BE INSTALLED. 6" LONG HEADLOK SCREWS SHALL BE USED INSTEAD OF NEW LAGS IN THIS INSTANCE.

DRIVE NAIL AND SPIKE POPS

- NUMEROUS EXISTING NAIL AND SPIKE HEADS PROTRUDE PROUD OF THEIR SUBSTRATE. THE PROTRUDING HEADS ARE SCATTERED ALONG THE LENGTH OF THE BOARDWALK SPANNING DRAWINGS S-2 THROUGH S-9 WITH GREATEST CONCENTRATION IN THE VICINITY SHOWN IN DRAWINGS S-3 AND S-4. THE PROTRUDING HEADS ARE OBSERVED ON THE DECKING AND ON THE GUARDRAILS. THE HEADS CAN BE ACCESSED BY WALKING ALONG THE BOARDWALK AND DO NOT REQUIRE CLIMBING LADDERS OR STANDING OUTSIDE OF THE WALKING PATH TO ACCESS. THE PROTRUDING HEADS MAY HAVE BEEN CAUSED BY THERMAL AND WOOD MOISTURE CONTENT FLUCTUATIONS OVER THE LIFE OF THE TIMBER STRUCTURE, MAY BE FROM UNDERDRIVING OF FASTENERS DURING ORIGINAL CONSTRUCTION OR DEFLECTION OF THE STRUCTURE OVER THE YEARS.
- CONTRACTOR SHALL DRIVE FLUSH ALL NAIL AND SPIKE HEADS THAT ARE STICKING UP PROUD OF THE WOOD SUBSTRATE SURFACE ON THE TIMBER BOARDWALK, GUARD RAILS, INTERMEDIATE FLOATS, TIMBER MEMBERS OF D FLOATS AND E FLOATS. THE TIMBER BOARDWALK SPANS FROM DRAWING S-2 THROUGH S-9.
- A SPRING IMPACT NAIL SET BY SPRING TOOLS® OR EQUIVALENT SHALL BE USED FOR NAILS. KEEP NAIL HEADS, ESPECIALLY TOENAILS, FROM BEING OVERDRIVEN INTO WOOD SUBSTRATE IF HEAVILY WEATHERED OR THIN WOOD BOARDS TO REDUCE THE CHANCE OF SPLITTING.
- NAIL AND SPIKE POPS TO BE DRIVEN AS PART OF OTHER REPAIRS SHOWN IN THESE DRAWINGS SHALL NOT COUNT TOWARD THE QUANTITY OF REPAIR R102, "DRIVE NAIL AND SPIKE POPS" STATED IN THE REPAIR SCHEDULE

ONE INCH
↑
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

FOR BID



REVISIONS				
REV	DATE	BY	APPD	DESCRIPTION

DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
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**PERCIVAL LANDING
2021 REPAIRS**

GENERAL NOTES

G-2

SHEET NO. 2 OF 21

GENERAL NOTES CONT.

WOOD FASTENER INSTALLATION

- ALL THREADED FASTENERS INSTALLED BY ROTATIONAL DRIVING MEANS ARE CONSIDERED SCREWS IN THIS CONTRACT, UNO, FOR EXAMPLE, WOOD SCREWS, LAG SCREWS, POWER LAG, LEDGERLOK, HEADLOK, SPAX, ETC.
- INSTALL SCREWS SUCH THAT SCREW HEADS FULLY BEAR AGAINST SUBSTRATE. NAILS AND SPIKES SHALL BE DRIVEN FLUSH WITH SUBSTRATE. MATTING SUBSTRATES SHALL BE HELD IN FULL CONTACT BY FASTENERS.
- WHEN INSTALLING FASTENERS, PROVIDE FULL CONTACT BETWEEN MATTING SUBSTRATES BY MEANS OTHER THAN THE FORCE DELIVERED FROM THE FASTENER DRIVING TO MINIMIZE THE CHANCE OF SPIN-OUT/STRIPPING OF THE FASTENER HOLE.
- NO SCREW HEADS SHALL BE STRIPPED DURING DRIVING, AS COATING MAY BECOME DAMAGED. ANY STRIPPED SCREW HEADS, AS DETERMINED BY THE ENGINEER, SHALL BE REMOVED AND REPLACED WITH A NEW SCREW.
- OVERDRIVING FASTENERS IS NOT ACCEPTABLE. WOOD MEMBERS, EXISTING OR NEW, THAT THE CONTRACTOR HAS OVERDRIVEN FASTENERS INTO MAY BE DETERMINED TO BE DAMAGED BY THE ENGINEER AND IN WHICH CASE SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- WASHERS SHALL BE USED UNDER ALL BOLT HEADS, NUTS AND LAG SCREW HEADS, EXCEPT ECONOMY BOLT HEADS.

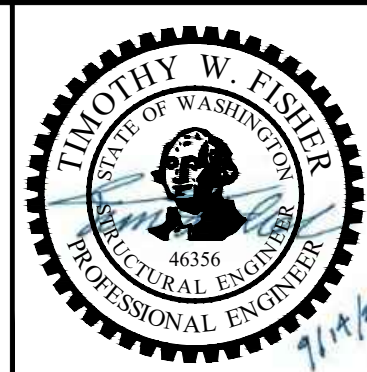
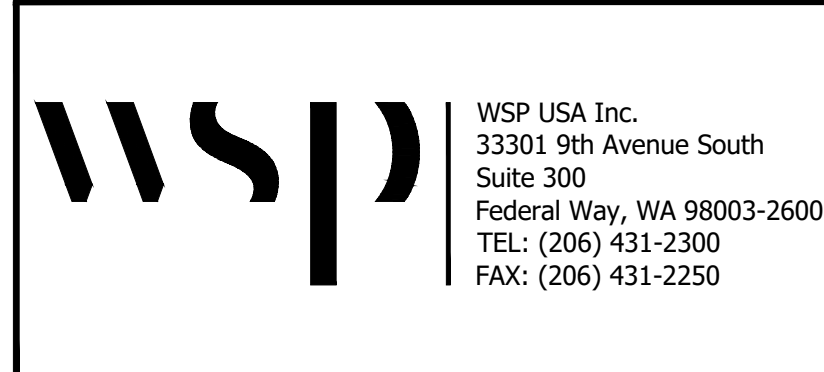
PLASTIC LUMBER

- PLASTIC BULLRAIL AND PLASTIC SCUPPER BLOCKS SHALL BE FIBER REINFORCED MOLDED LUMBER, HDPE WITH FIBERGLASS FILAMENT, BY TENGENT OR EQUIVALENT. MATERIAL SHALL BE UV STABILIZED.
- WHERE DAPPING AND NOTCHING FOR LAPS OR OTHER CONNECTIONS ARE MADE IN PLASTIC LUMBER, OVERCUTTING IS NOT ALLOWED, AS NOT TO MAKE A WEAK POINT IN MEMBER WHERE A CRACK OR FRACTURE COULD EMINATE. ALL TRANSITIONS FROM ONE CUT PLANE TO ANOTHER SHALL BE SMOOTH AND FREE OF NOTCHES OR ABRUPT EDGES, LOCAL SANDING OR SHAPING OF CUT SURFACES MAY BE REQUIRED.
- MATCH SIZE AND COLOR WITH EXISTING PLASTIC AND SUBMIT SAMPLES TO ENGINEER FOR APPROVAL.
- THE SAME REQUIREMENTS FOR WOOD/TIMBER QUALITY WORKMANSHIP APPLY TO PLASTIC LUMBER AS WELL.
- HARDWARE SHALL BE PROVIDED THAT MATCHES THE TYPICAL HARDWARE USED FOR SIMILAR CONNECTIONS AT E FLOAT, UNO.

REPAIR SCHEDULE					
SHEET NO.	REPAIR DESIGNATION *	DESCRIPTION	NUMBER OF LOCATIONS	PROJECT QUANTITY	QUANTITY UNITS
S-2	R1	REPAIR/REPLACE BROKEN/MISSING BENCH TRIM	3	-	-
S-2	R2	REPLACE DETERIORATED BENCH	1	-	-
S-2	R3	REPLACE MISSING CARRIAGE BOLT NUT AND WASHER	3	-	-
S-2	R4	TIGHTEN LOOSE CARRIAGE BOLT NUT	3	-	-
S-2, S-3, S-4, S-5, S-6, S-7, S-8, S-9	R102	DRIVE NAIL AND SPIKE POPS	MANY	100 ESTIMATED	EA
S-3	R1	GUARDRAIL UPPER RAIL REPAIR	1	-	-
S-3	R2	TIGHTEN GUARDRAIL POST HARDWARE	1	-	-
S-4	R1	REPLACE MISSING GATE AND SELF-CLOSER TORSION SPRING	1	-	-
S-4	R2	REPLACE TIMBER DECKING BOARD **	1	-	-
S-5, S-6, S-7, S-8, S-9	R100	REPLACE BOARDWALK TIMBER DECKING BOARDS	MANY	240	LF
S-5	R1	REPLACE BROKEN/ROTTED TIMBER HORIZONTAL BRACE MEMBER	3	-	-
S-5	R2	TIGHTEN GUARDRAIL POST HARDWARE	1	-	-
S-5	R3	REPLACE MISSING GATE AND SELF-CLOSER TORSION SPRING	1	-	-
S-6	R1	REPLACE MISSING STEEL SECURITY GATE	1	-	-
S-6, S-7, S-8, S-9	R101	SHIM BOARDWALK TIMBER DECKING BOARDS	MANY	295	LF
S-7	R1	ADD TIMBER BLOCKING AND FASTEN FOR GUARDRAIL POSTS AT OUTER STRINGER	3	-	-
S-7	R2	FASTEN OUTER STRINGER END	1	-	-
S-7	R3	DRIVE PULLED UP OUTER STRINGER SPIKES	1	-	-
S-7	R4	REPLACE GUARDRAIL POST	2	-	-
S-7	R5	REPLACE ROTTED DECKING BOARD NEAR CURB **	1	-	-
S-8	R1	SHIM POST THAT IS NOT BEARING	1	-	-
S-8	R2	REPLACE POST AND FOOTING	1	-	-
S-8	R3	SHIM STRINGER THAT IS NOT BEARING	1	-	-
S-9	R1	REPAIR SPLIT STRINGER	1	-	-
S-10	R1	FASTEN LOOSE FLOAT RUB BOARD	7	-	-
S-10	R2	REPLACE SPLIT/MISSING FLOAT RUB BOARD	10	-	-
S-10	R3	BEVEL POST BOTTOMS AND NOTCH WALERS	1	-	-
S-10	R4	FASTEN LOOSE RIM JOIST	1	-	-
S-10	R5	PATCH CHAINLINK MESH	1	-	-
S-10	R6	REPLACE MISSING CARRIAGE BOLT NUT AND WASHER	8	-	-
S-10	R7	REPLACE MISSING PLASTIC SCUPPER BLOCKS	9	-	-
S-10	R8	REPLACE DAMAGED PLASTIC BULLRAIL MEMBERS	8	-	-
S-10	R9	PLACE THIN GROUT PAD	1	-	-
S-10	R10	FASTEN GANGWAY TRANSITION PLATE NOSING	1	-	-
S-10	R11	ADD SUPPLEMENTAL FLOTATION DRUMS	34	-	-

* SHOWN CIRCLED WHERE APPEARING ON DWG PLAN SHEET CALLOUTS.

** TIMBER DECKING BOARDS REPLACED FOR THIS ITEM COUNT TOWARDS THE 240 LF TOTAL SHOWN FOR REPAIR ITEM R100.



REVISIONS				
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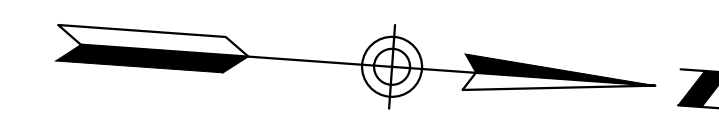
GENERAL NOTES AND REPAIR SCHEDULE

FOR BID

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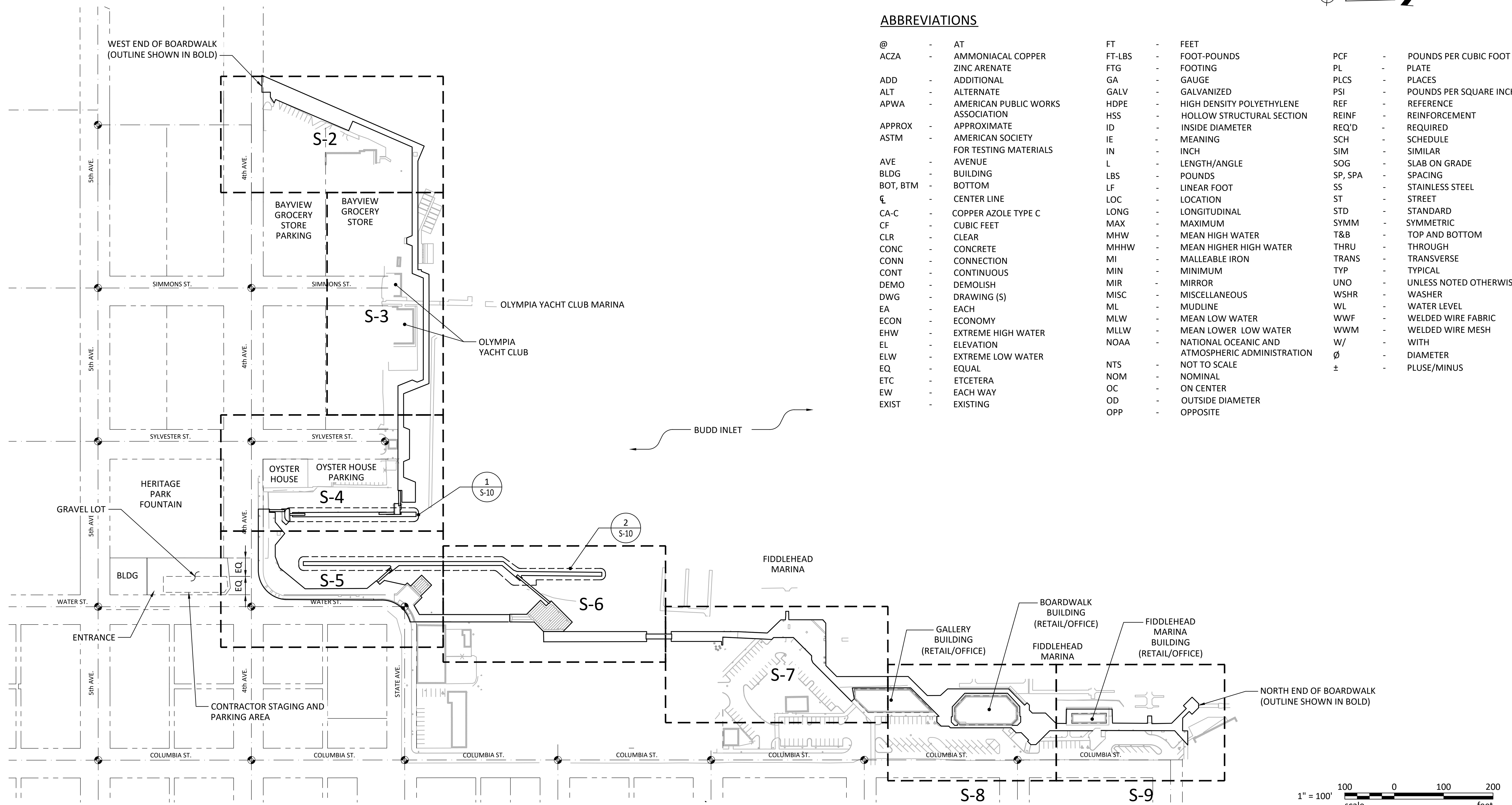
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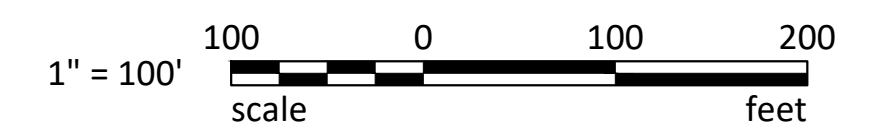


ABBREVIATIONS

@	-	AT	FT	-	FEET	PCF	-	POUNDS PER CUBIC FOOT
ACZA	-	AMMONIACAL COPPER ZINC ARENATE	FT-LBS	-	FOOT-POUNDS	PL	-	PLATE
ADD	-	ADDITIONAL	FTG	-	FOOTING	PLCS	-	PLACES
ALT	-	ALTERNATE	GA	-	GAUGE	PSI	-	POUNDS PER SQUARE INCH
APWA	-	AMERICAN PUBLIC WORKS ASSOCIATION	GALV	-	GALVANIZED	REF	-	REFERENCE
APPROX	-	APPROXIMATE	HDPE	-	HIGH DENSITY POLYETHYLENE	REINF	-	REINFORCEMENT
ASTM	-	AMERICAN SOCIETY FOR TESTING MATERIALS	HSS	-	HOLLOW STRUCTURAL SECTION	REQ'D	-	REQUIRED
AVE	-	AVENUE	ID	-	INSIDE DIAMETER	SCH	-	SCHEDULE
BLDG	-	BUILDING	IE	-	MEANING	SIM	-	SIMILAR
BOT, BTM	-	BOTTOM	IN	-	INCH	SOG	-	SLAB ON GRADE
CL	-	CENTER LINE	L	-	LENGTH/ANGLE	SP, SPA	-	SPACING
CA-C	-	COPPER AZOLE TYPE C	LBS	-	POUNDS	SS	-	STAINLESS STEEL
CF	-	CUBIC FEET	LF	-	LINEAR FOOT	ST	-	STREET
CLR	-	CLEAR	LOC	-	LOCATION	STD	-	STANDARD
CONC	-	CONCRETE	LONG	-	LONGITUDINAL	SYMM	-	SYMMETRIC
CONN	-	CONNECTION	MAX	-	MAXIMUM	T&B	-	TOP AND BOTTOM
CONT	-	CONTINUOUS	MHW	-	MEAN HIGH WATER	THRU	-	THROUGH
DEMO	-	DEMOLISH	MHHW	-	MEAN HIGHER HIGH WATER	TRANS	-	TRANSVERSE
DWG	-	DRAWING (S)	MI	-	MALLEABLE IRON	TYP	-	TYPICAL
EA	-	EACH	MIN	-	MINIMUM	UNO	-	UNLESS NOTED OTHERWISE
ECON	-	ECONOMY	MIR	-	MIRROR	WSHR	-	WASHER
EHW	-	EXTREME HIGH WATER	MISC	-	MISCELLANEOUS	WL	-	WATER LEVEL
EL	-	ELEVATION	ML	-	MUDLINE	WWF	-	WELDED WIRE FABRIC
ELW	-	EXTREME LOW WATER	MLW	-	MEAN LOW WATER	WWW	-	WELDED WIRE MESH
EQ	-	EQUAL	MLLW	-	MEAN LOWER LOW WATER	W/	-	WITH
ETC	-	ETCETERA	NOAA	-	NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION	∅	-	DIAMETER
EW	-	EACH WAY	NTS	-	NOT TO SCALE	±	-	PLUSE/MINUS
EXIST	-	EXISTING	NOM	-	NOMINAL			
			OC	-	ON CENTER			
			OD	-	OUTSIDE DIAMETER			
			OPP	-	OPPOSITE			



OVERALL SITE PLAN
SCALE: 1" = 100'



ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

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REV	DATE	BY	APPD	DESCRIPTION

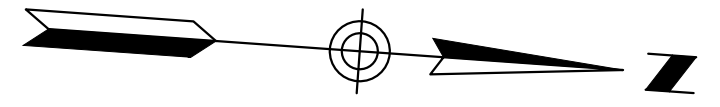
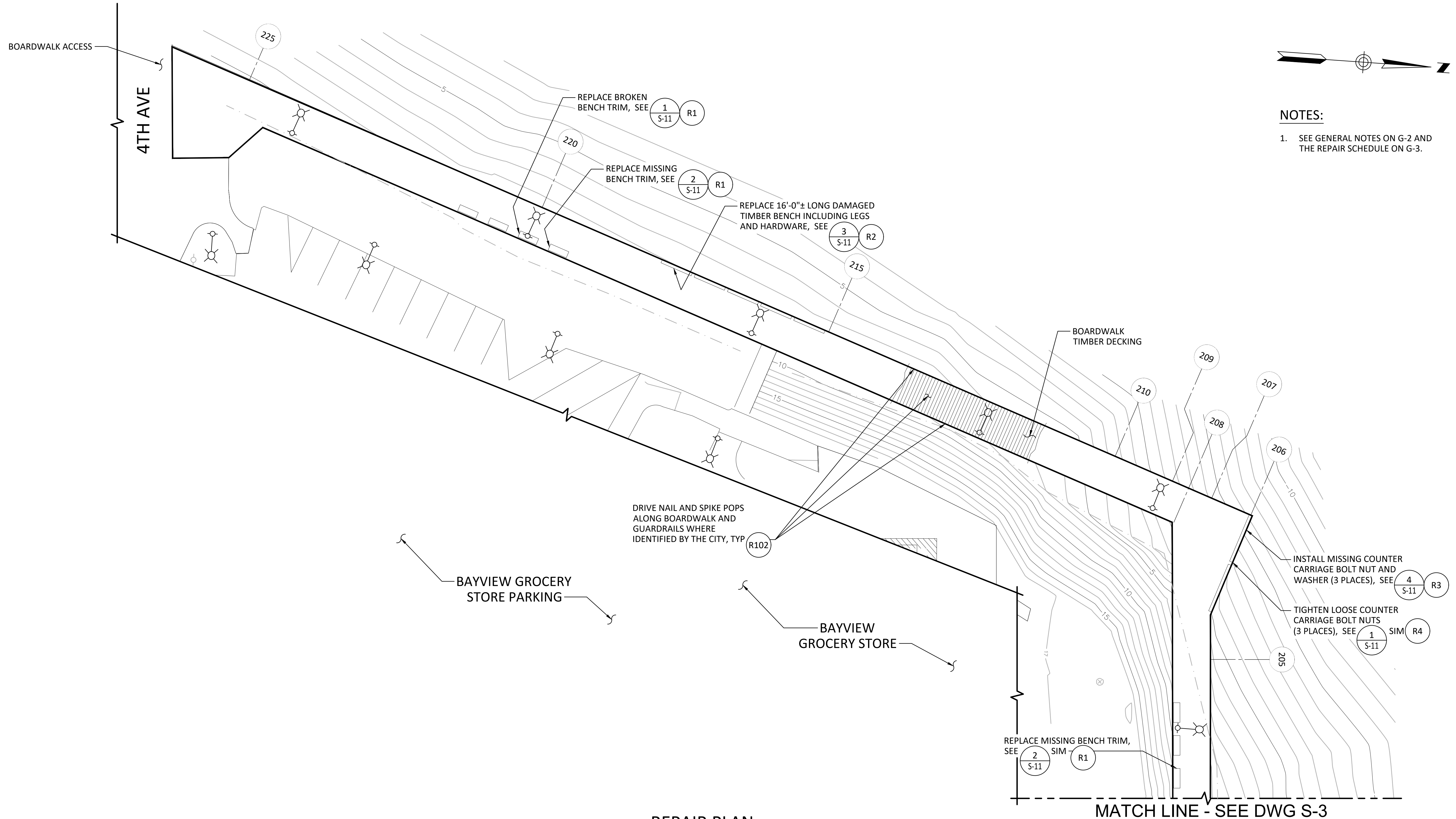
DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
SCALE: AS SHOWN
DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

**OVERALL SITE PLAN, ABBREVIATIONS,
CONTRACTOR STAGING AND PARKING**

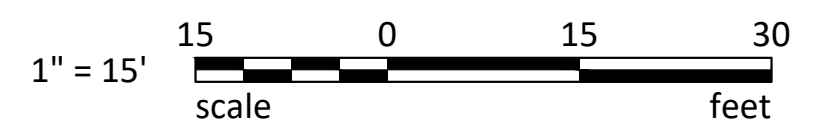
S-1

SHEET NO. **4** OF **21**



- NOTES:**
- SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.

REPAIR PLAN
SCALE: 1" = 15'-0"



ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

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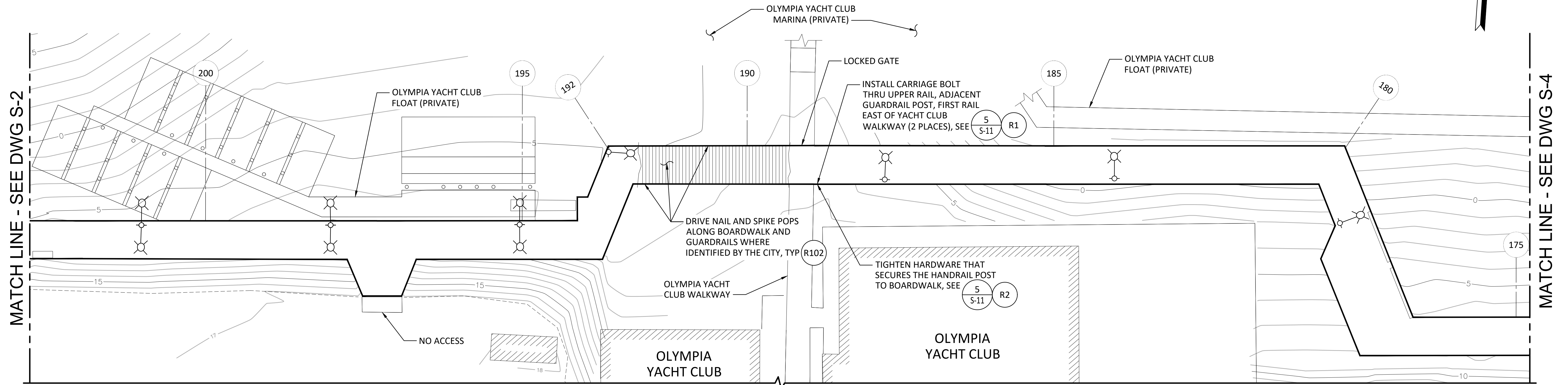
DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
SCALE: AS SHOWN
DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PLAN - SHEET 1

S-2

SHEET NO. **5** OF **21**

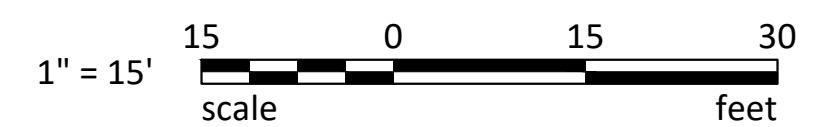


NOTES:

1. SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.



REPAIR PLAN
SCALE: 1" = 15'-0"



ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

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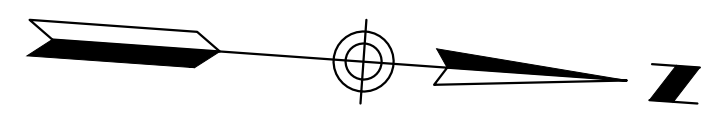
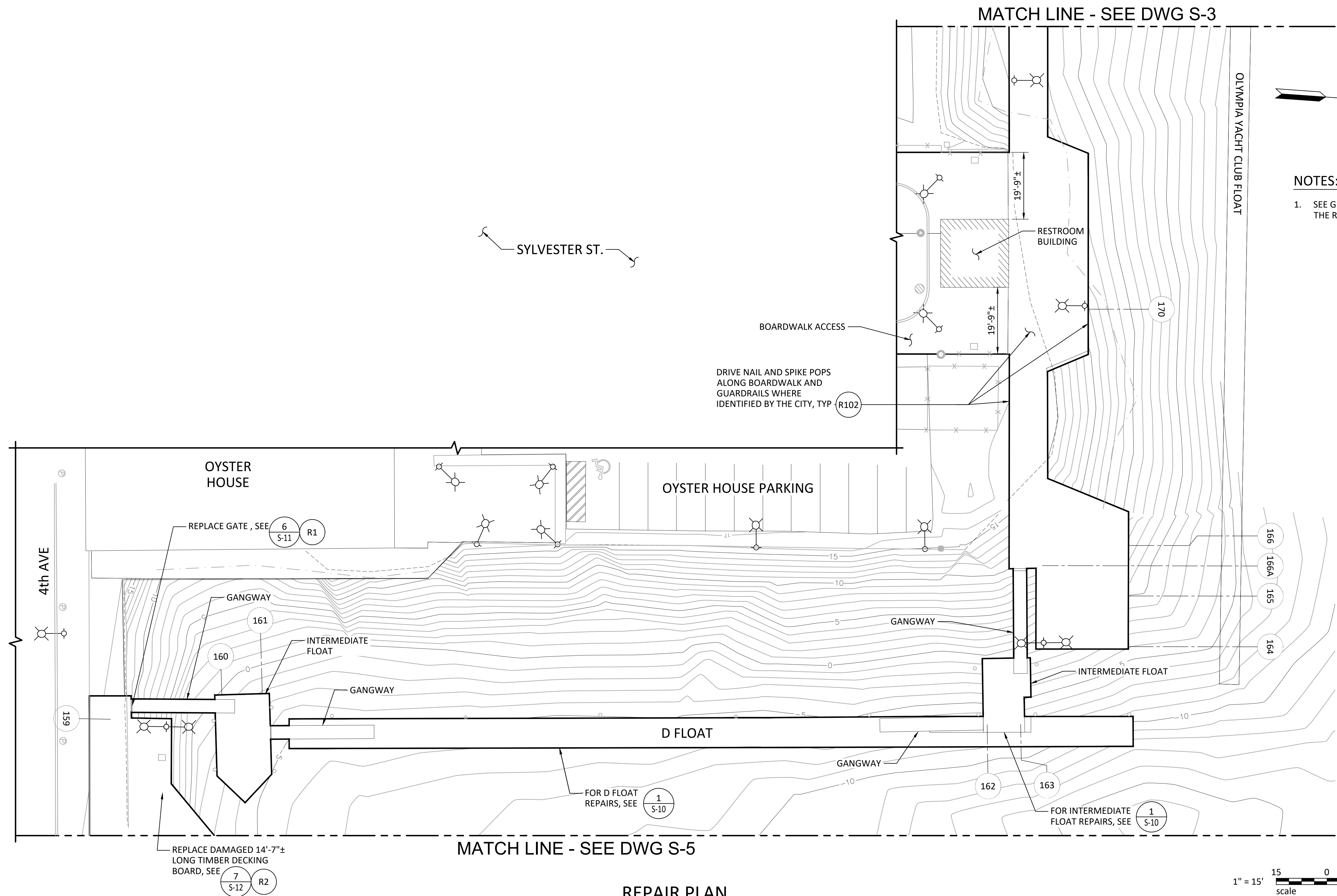
DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
SCALE: AS SHOWN
DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PLAN - SHEET 2

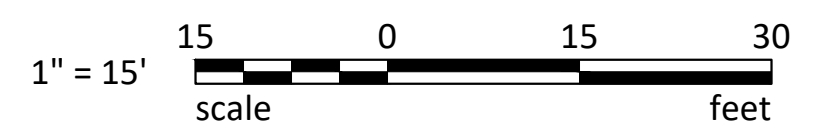
S-3

SHEET NO. **6** OF **21**



NOTES:
 1. SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.

REPAIR PLAN
 SCALE: 1" = 15'-0"



ONE INCH
 AT FULL SIZE, IF NOT ONE
 INCH SCALE ACCORDINGLY

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REVISIONS				
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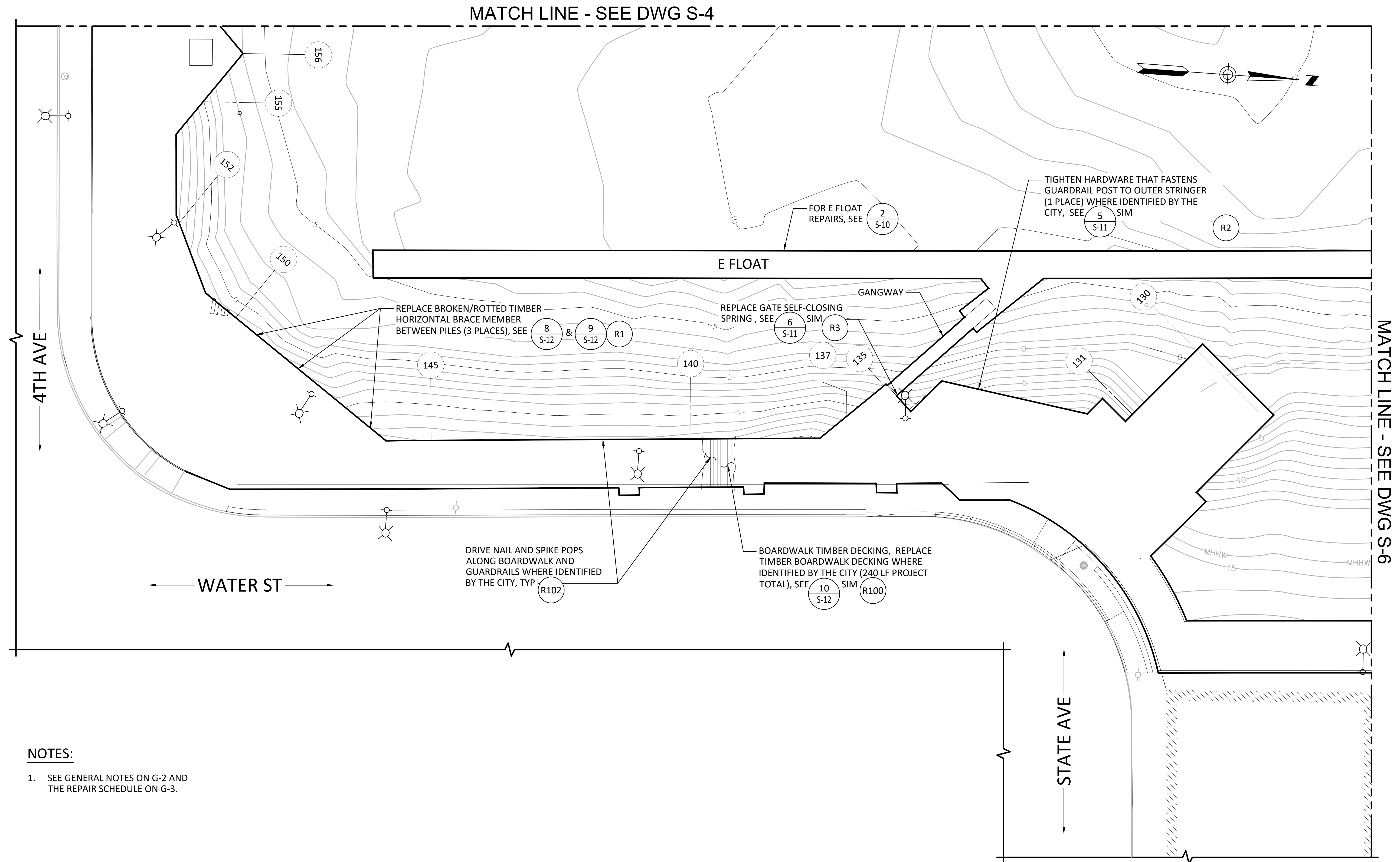
DESIGNED BY: TWF
 DRAWN BY: AHK
 CHECKED BY: JRG
 APPROVED BY: JRG
 SCALE: AS SHOWN
 DATE: September 14, 2021

**PERCIVAL LANDING
 2021 REPAIRS**

REPAIR PLAN - SHEET 3

S-4

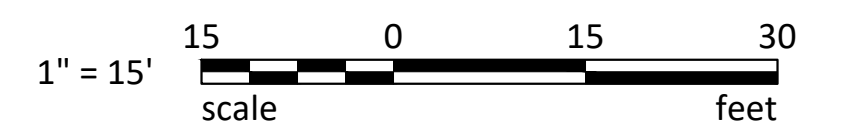
SHEET NO. 7 OF 21



NOTES:

- SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.

REPAIR PLAN
SCALE: 1" = 15'-0"



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SCALE: AS SHOWN
DATE: September 14, 2021

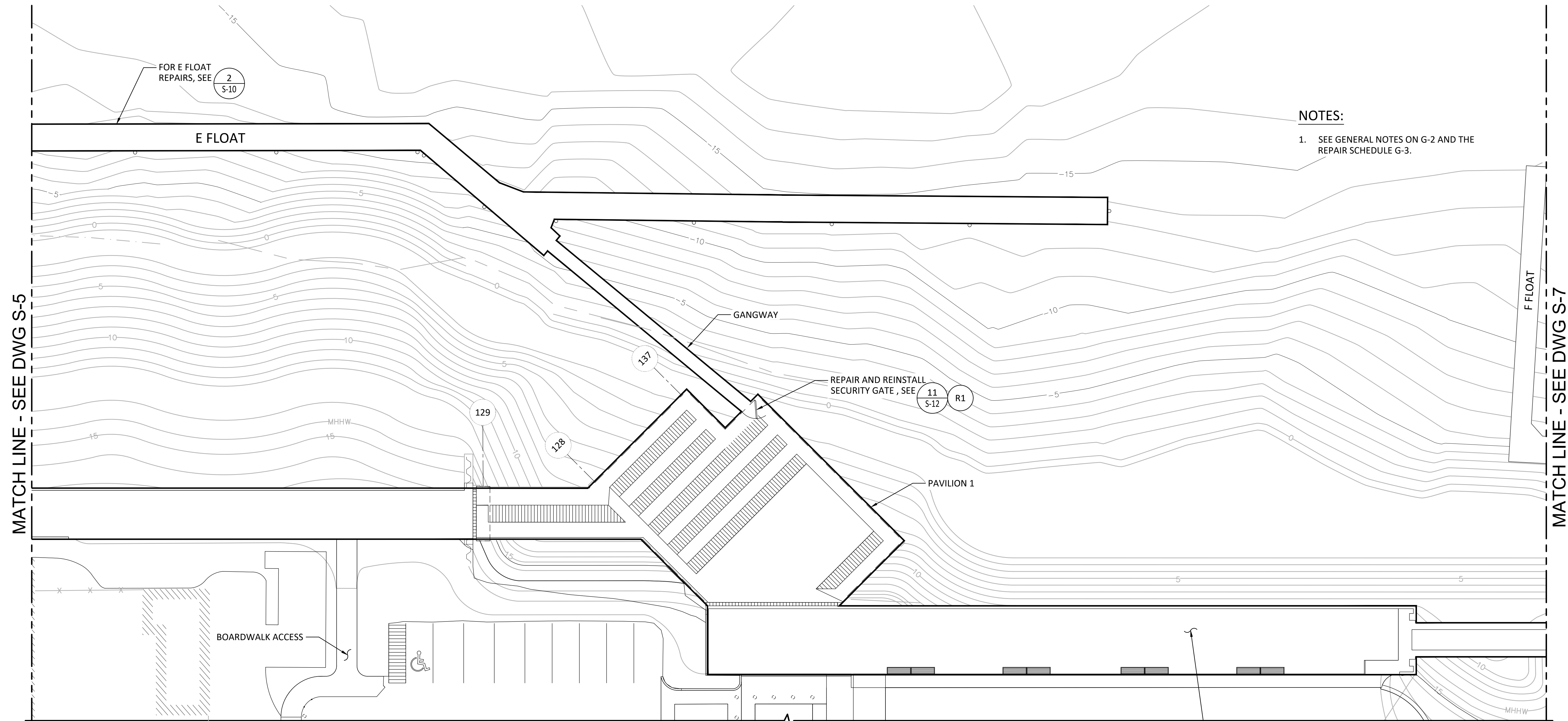
**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PLAN - SHEET 4

S-5

SHEET NO. **8** OF **21**

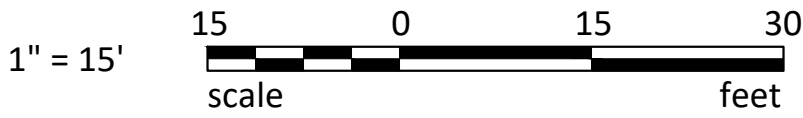
ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY



NOTES:
 1. SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE G-3.

BOARDWALK TIMBER DECKING, REPLACE AND/OR SHIM BOARDWALK TIMBER DECKING WHERE IDENTIFIED BY THE CITY. PROJECT TOTAL OF 295 LF OF EXISTING TIMBER DECKING TO BE SHIMMED AND 240 LF TO BE REPLACED. SEE 12 S-12 R100 R101

REPAIR PLAN
 SCALE: 1" = 15'-0"



ONE INCH
 AT FULL SIZE, IF NOT ONE
 INCH SCALE ACCORDINGLY

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REVISIONS				
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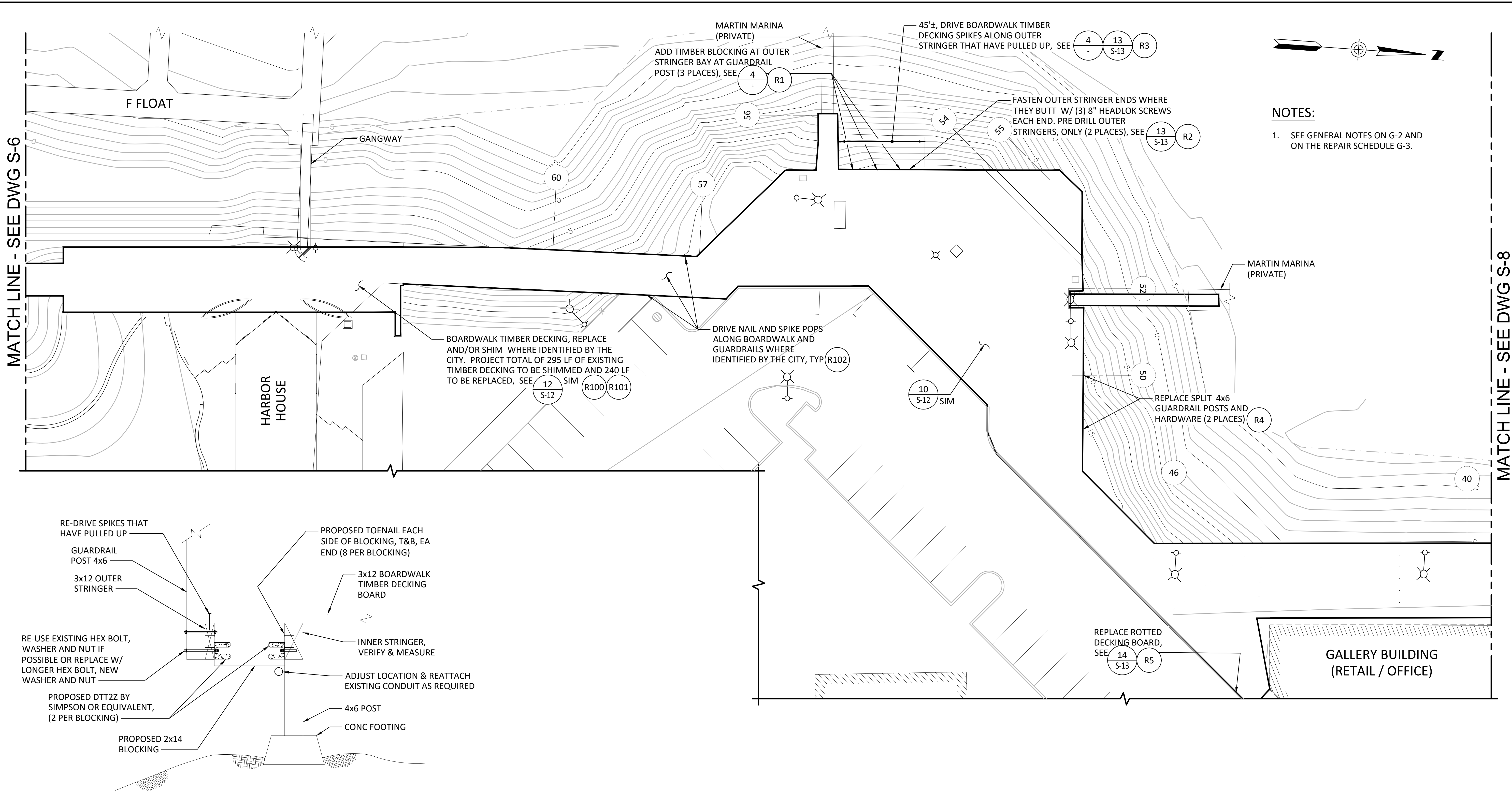
DESIGNED BY: TWF
 DRAWN BY: AHK
 CHECKED BY: JRG
 APPROVED BY: JRG
 SCALE: AS SHOWN
 DATE: September 14, 2021

**PERCIVAL LANDING
 2021 REPAIRS**

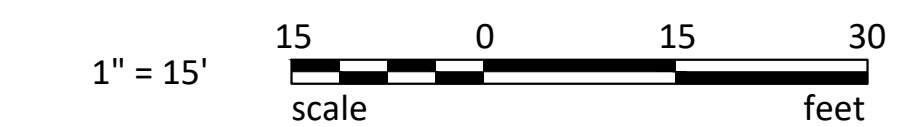
REPAIR PLAN - SHEET 5

S-6

SHEET NO. 9 OF 21



NOTES:
 1. SEE GENERAL NOTES ON G-2 AND ON THE REPAIR SCHEDULE G-3.



REPAIR PLAN
 SCALE: 1" = 15'-0"

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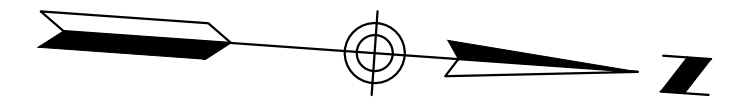
**PERCIVAL LANDING
 2021 REPAIRS**

REPAIR PLAN - SHEET 6

S-7

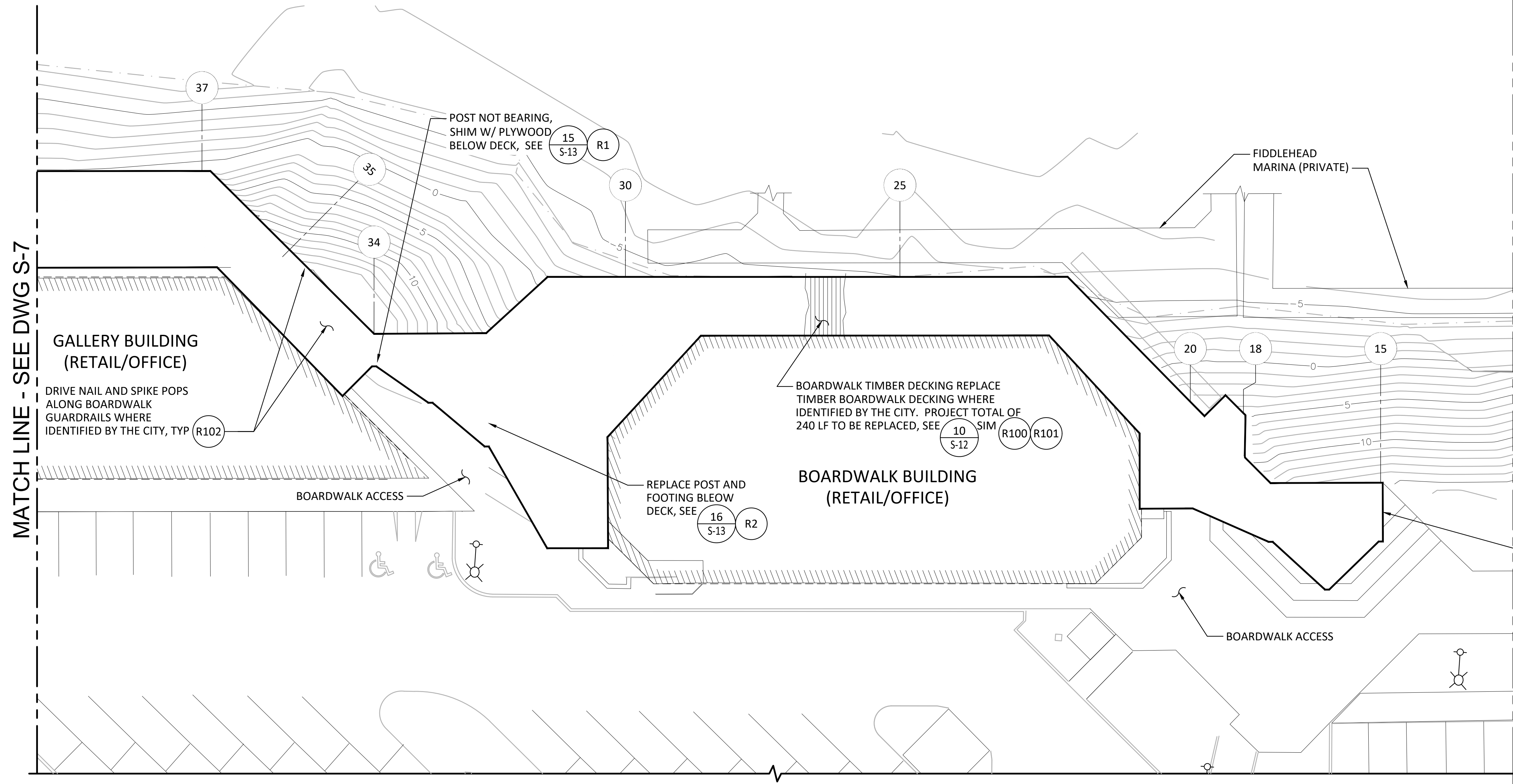
SHEET NO. **10** OF **21**

ONE INCH
 AT FULL SIZE, IF NOT ONE
 INCH SCALE ACCORDINGLY

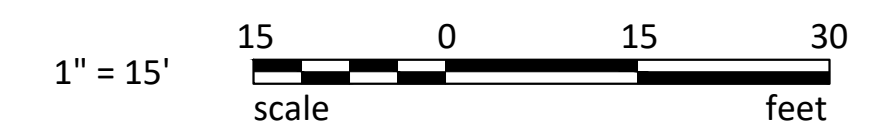


NOTES:

1. SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.



REPAIR PLAN
SCALE: 1" = 15'-0"



ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

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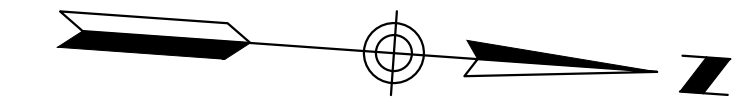
DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
SCALE: AS SHOWN
DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PLAN - SHEET 7

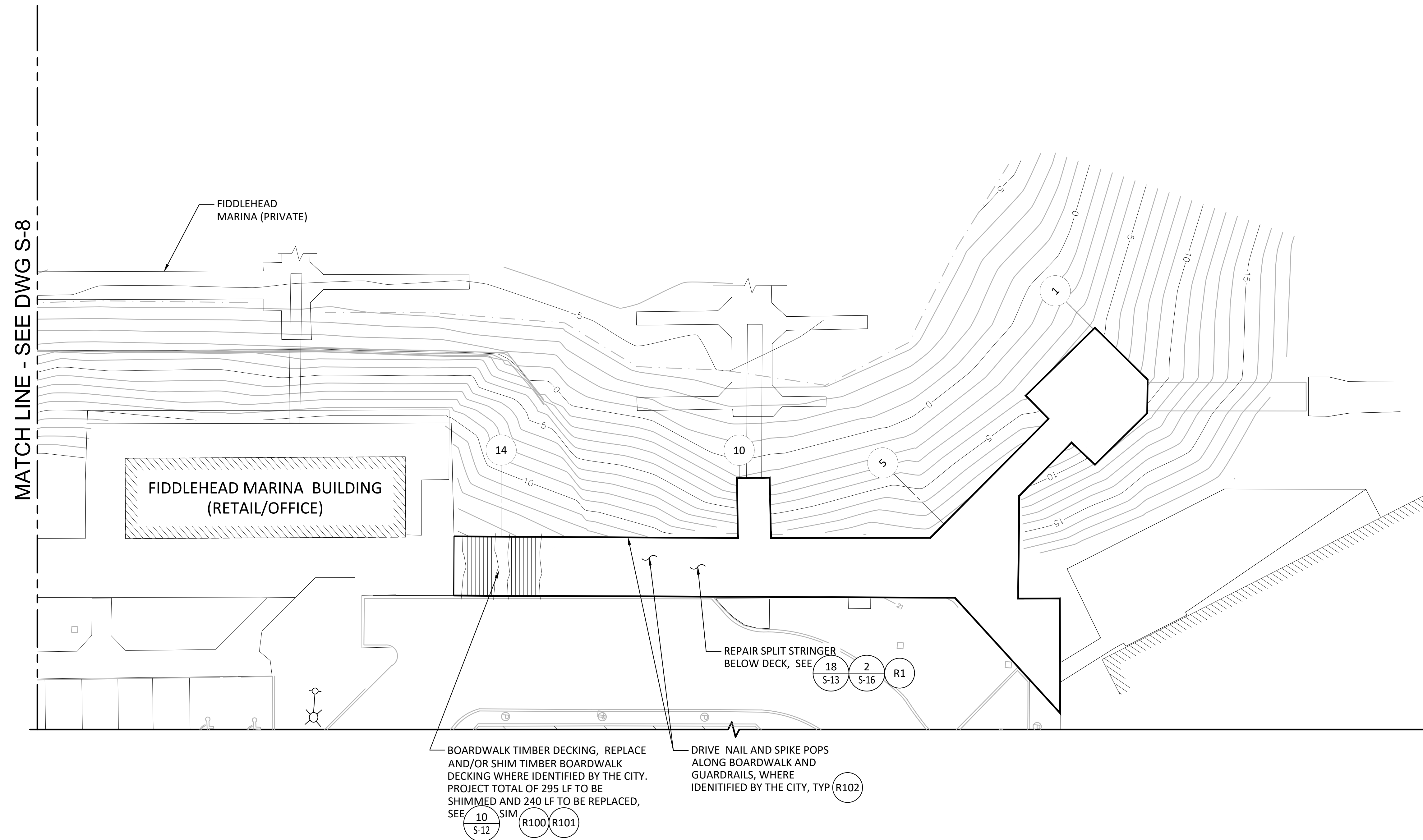
S-8

SHEET NO. **11** OF **21**

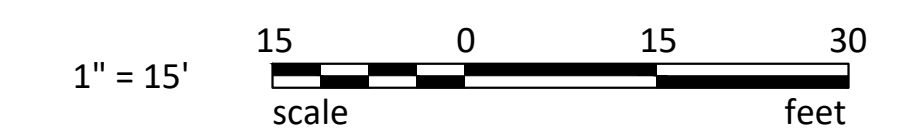


NOTES:

1. SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.



REPAIR PLAN
SCALE: 1" = 15'-0"



ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

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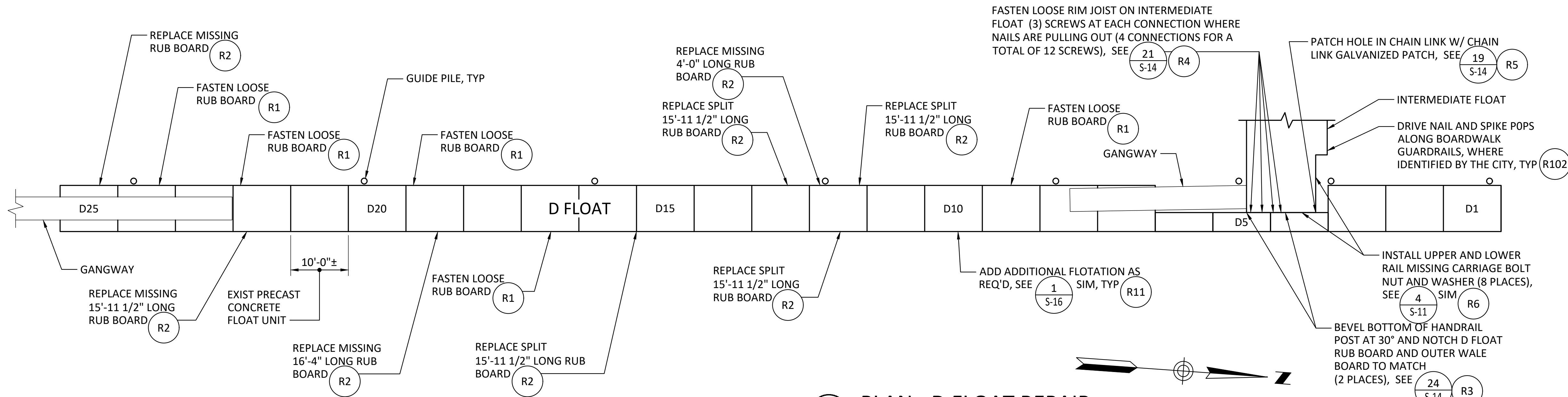
DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
SCALE: AS SHOWN
DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

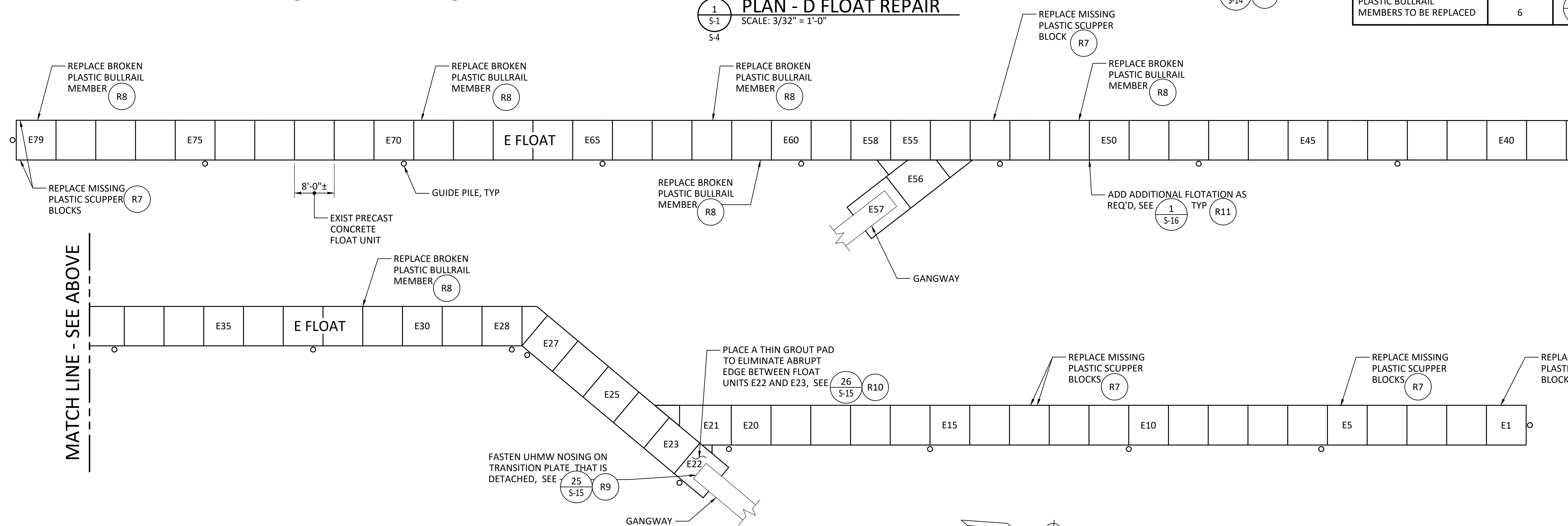
REPAIR PLAN - SHEET 8

S-9

SHEET NO. **12** OF **21**



1 PLAN - D FLOAT REPAIR
SCALE: 3/32" = 1'-0"



2 PLAN - E FLOAT REPAIR
SCALE: 3/32" = 1'-0"

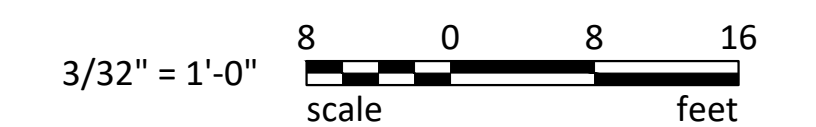
NOTES:

- SEE GENERAL NOTES ON G-2 AND THE REPAIR SCHEDULE ON G-3.
- REPLACEMENT OF (2) ADDITIONAL ±16' RUB BOARDS PLUS FASTENING OF (2) ADDITIONAL LOOSE ±16' RUB BOARDS BEYOND THOSE SHOWN IN THE TABLE BELOW SHALL BE ASSUMED.
- REPLACEMENT OF (2) ADDITIONAL PLASTIC SCUPPER BLOCKS AND (2) ADDITIONAL PLASTIC BULLRAIL MEMBERS BEYOND THOSE SHOWN IN THE TABLE BELOW SHALL BE ASSUMED.
- BOARD LENGTHS, WHERE STATED, ARE APPROXIMATE. CONTRACTOR SHALL MEASURE.

FLOATS REPAIR QUANTITIES	QUANTITY	DETAILS
LOOSE RUB BOARDS TO BE FASTENED	5	(3 S-16), (20 S-14) SIM
MISSING OR SPLIT RUB BOARDS TO BE REPLACES	8	(3 S-16), (20 S-14) SIM
MISSING PLASTIC SCUPPER BLOCKS TO BE REPLACED	7	(23 S-14) SIM
PLASTIC BULLRAIL MEMBERS TO BE REPLACED	6	(22 S-14) SIM

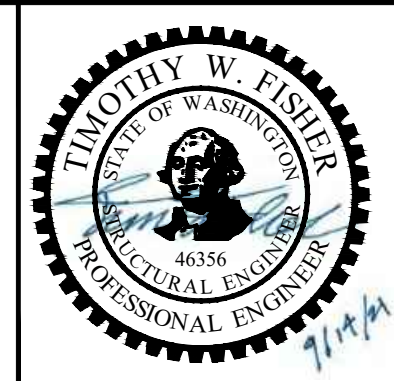
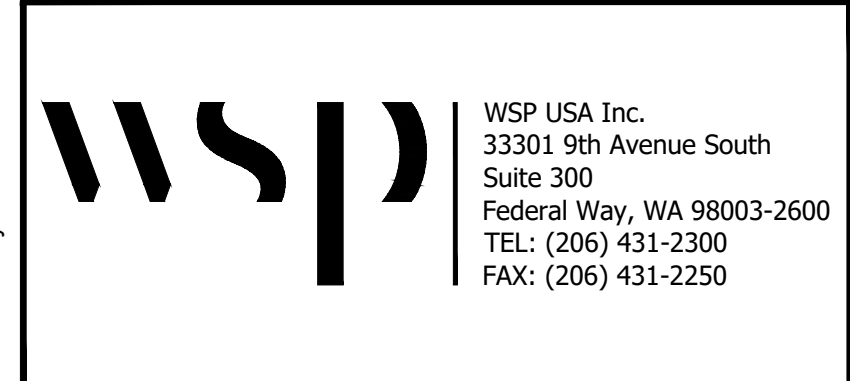
MATCH LINE - SEE ABOVE

MATCH LINE - SEE BELOW



ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

FOR BID



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APPROVED BY: JRG
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**PERCIVAL LANDING
2021 REPAIRS**

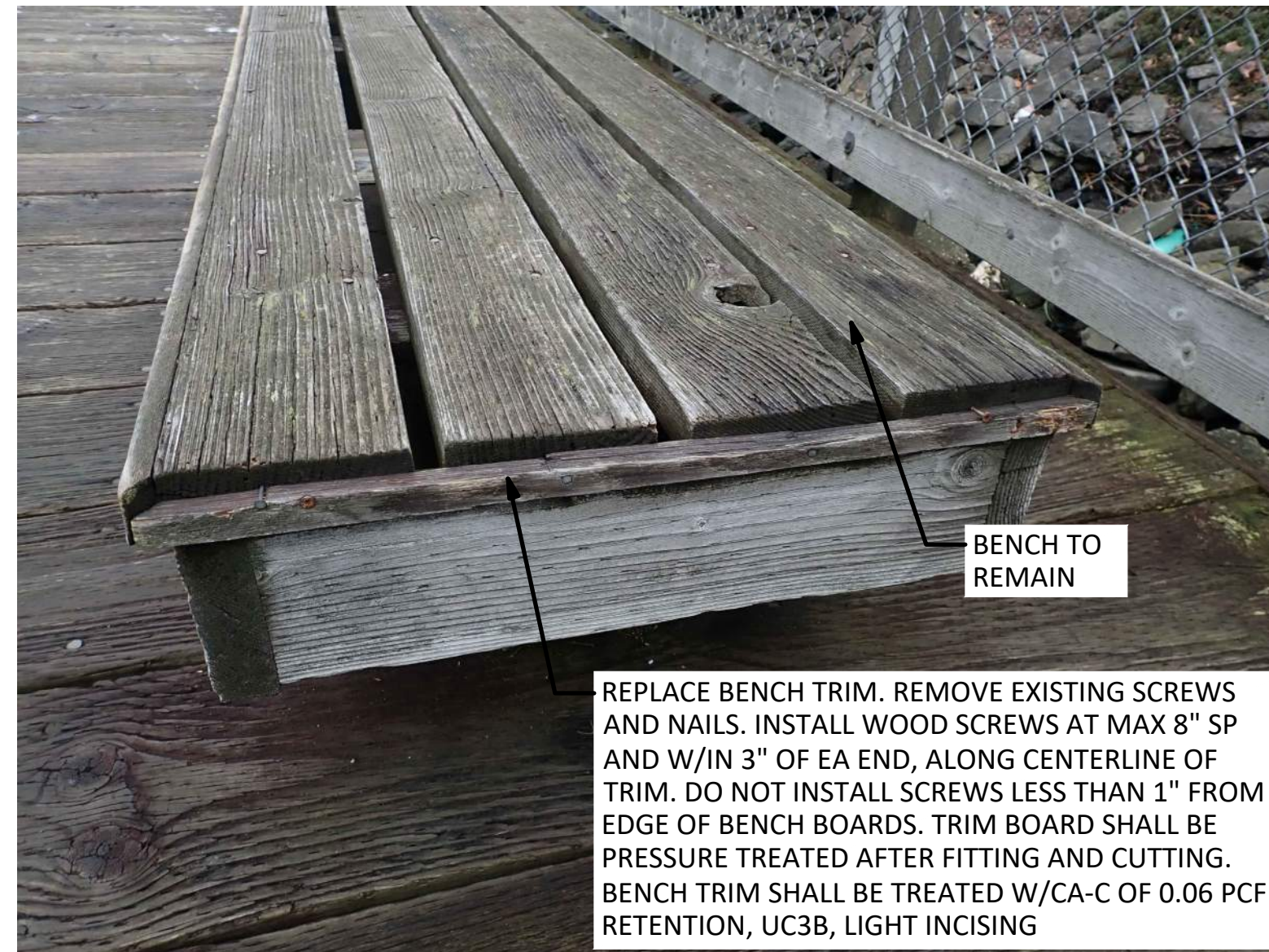
FLOAT REPAIR PLAN

S-10

SHEET NO. 13 OF 21

NOTES:

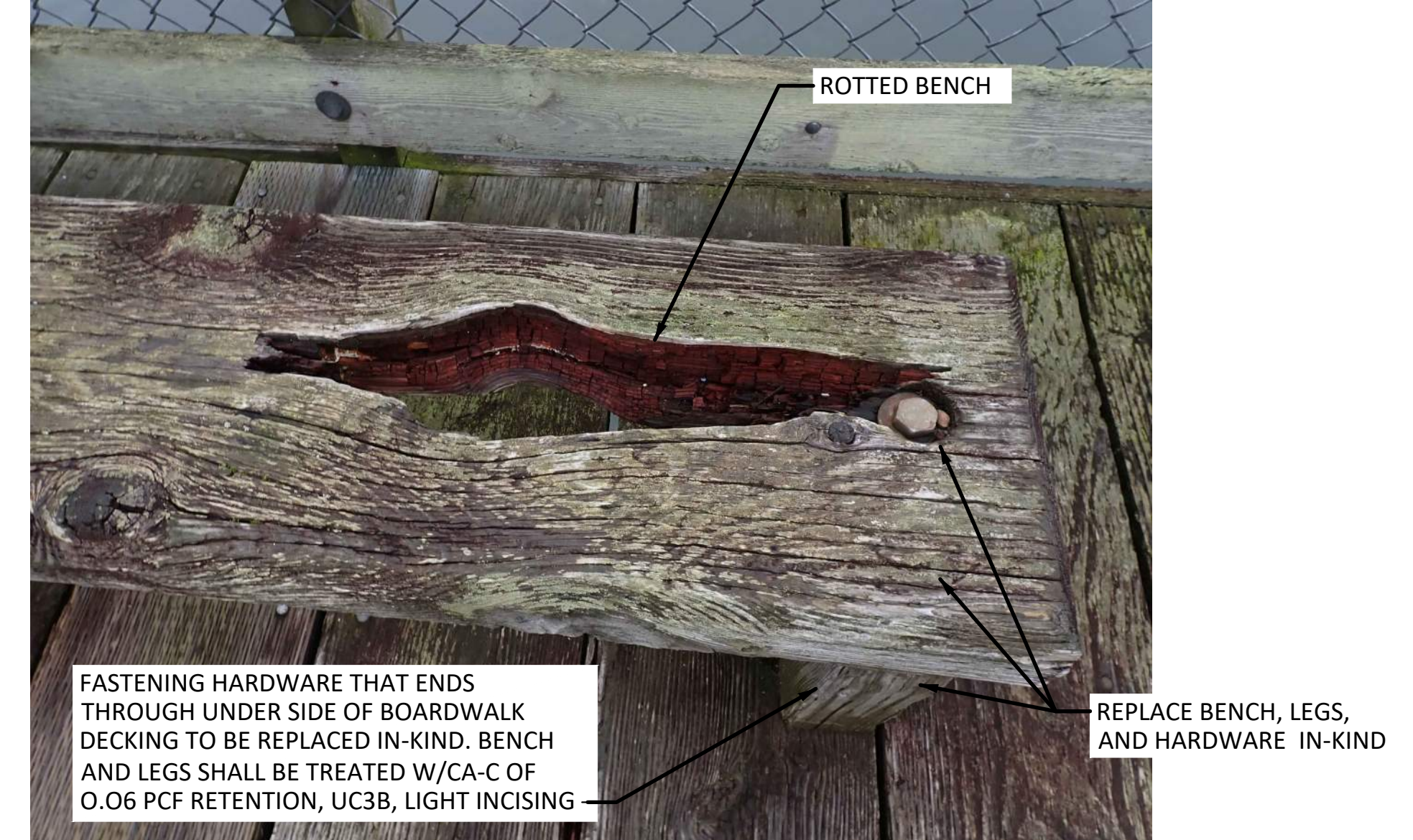
1. SEE GENERAL NOTES ON G-2 AND G-3.
2. DRAWING SHALL BE PRINTED IN COLOR.



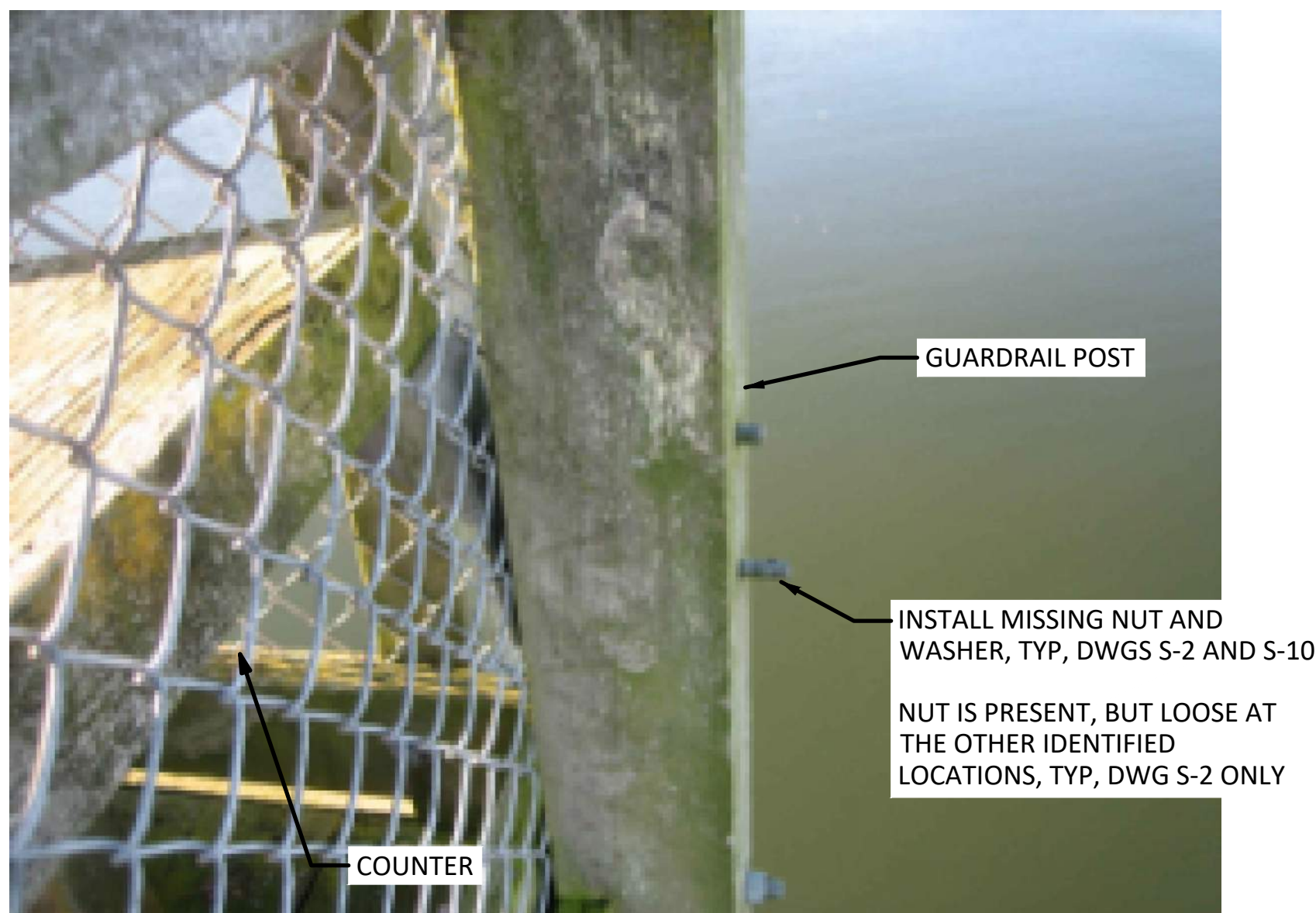
1 PHOTO - 1
S-2 SCALE: NTS



2 PHOTO - 2
S-2 SCALE: NTS



3 PHOTO - 3
S-2 SCALE: NTS



4 PHOTO - 4
S-2, S-10 SCALE: NTS



5 PHOTO - 5
S-3, S-5 SCALE: NTS



6 PHOTO - 6
S-4, S-5 SCALE: NTS

ONE INCH
AT FULL SIZE. IF NOT ONE
INCH SCALE ACCORDINGLY

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REV	DATE	BY	APPD	DESCRIPTION

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APPROVED BY: JRG
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**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PHOTOS - SHEET 1

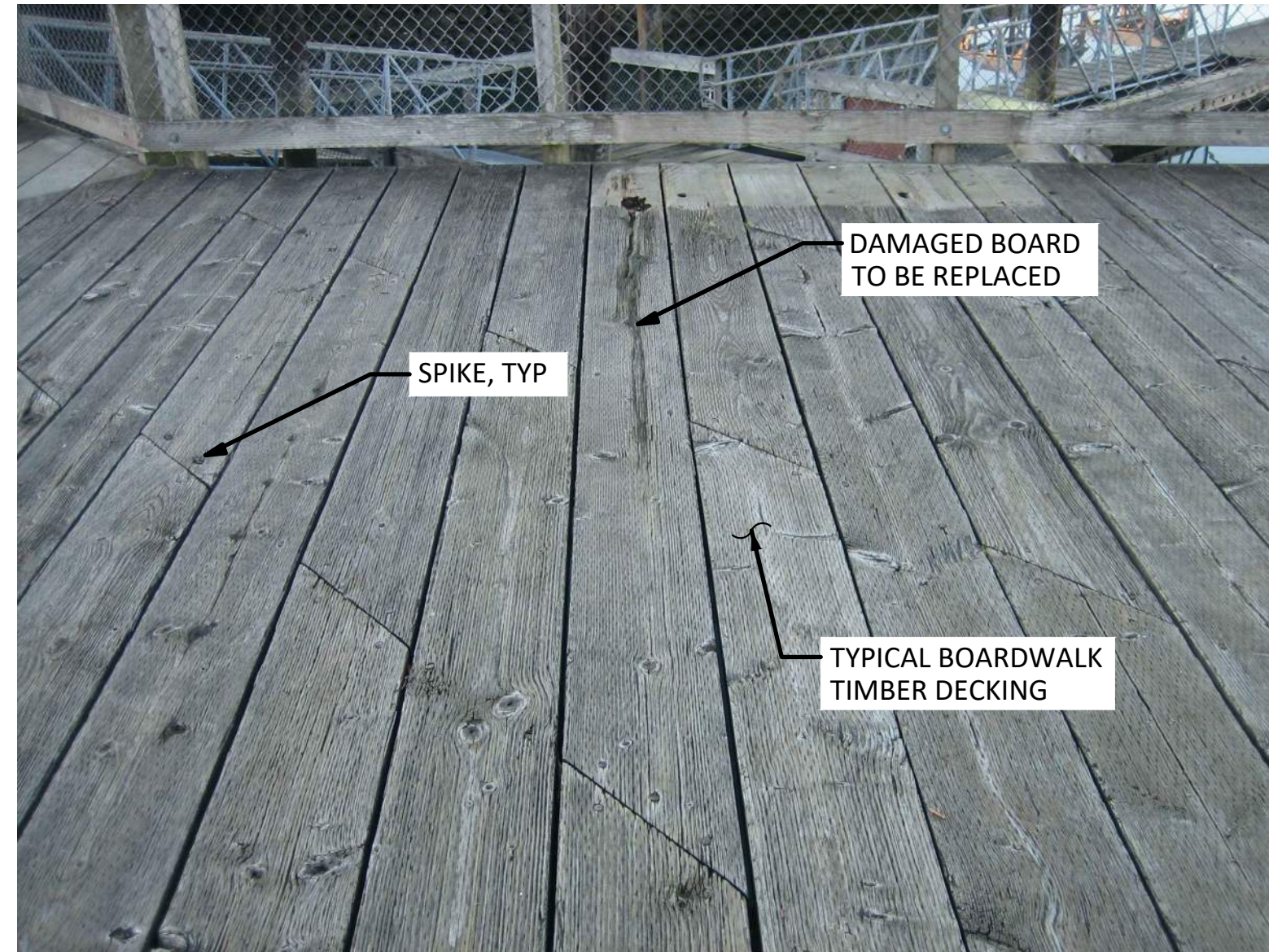
S-11

SHEET NO. 14 OF 21

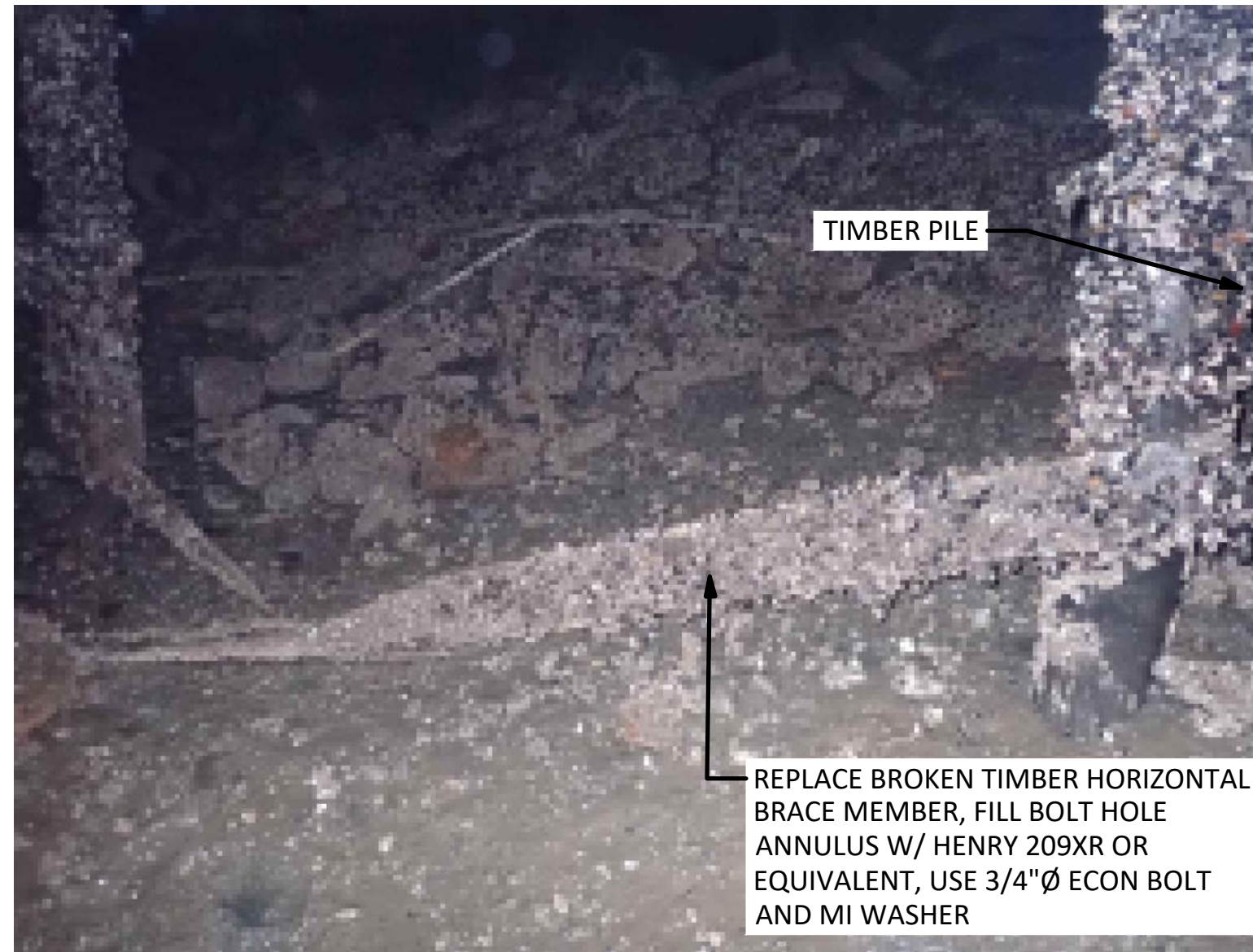
FOR BID

NOTES:

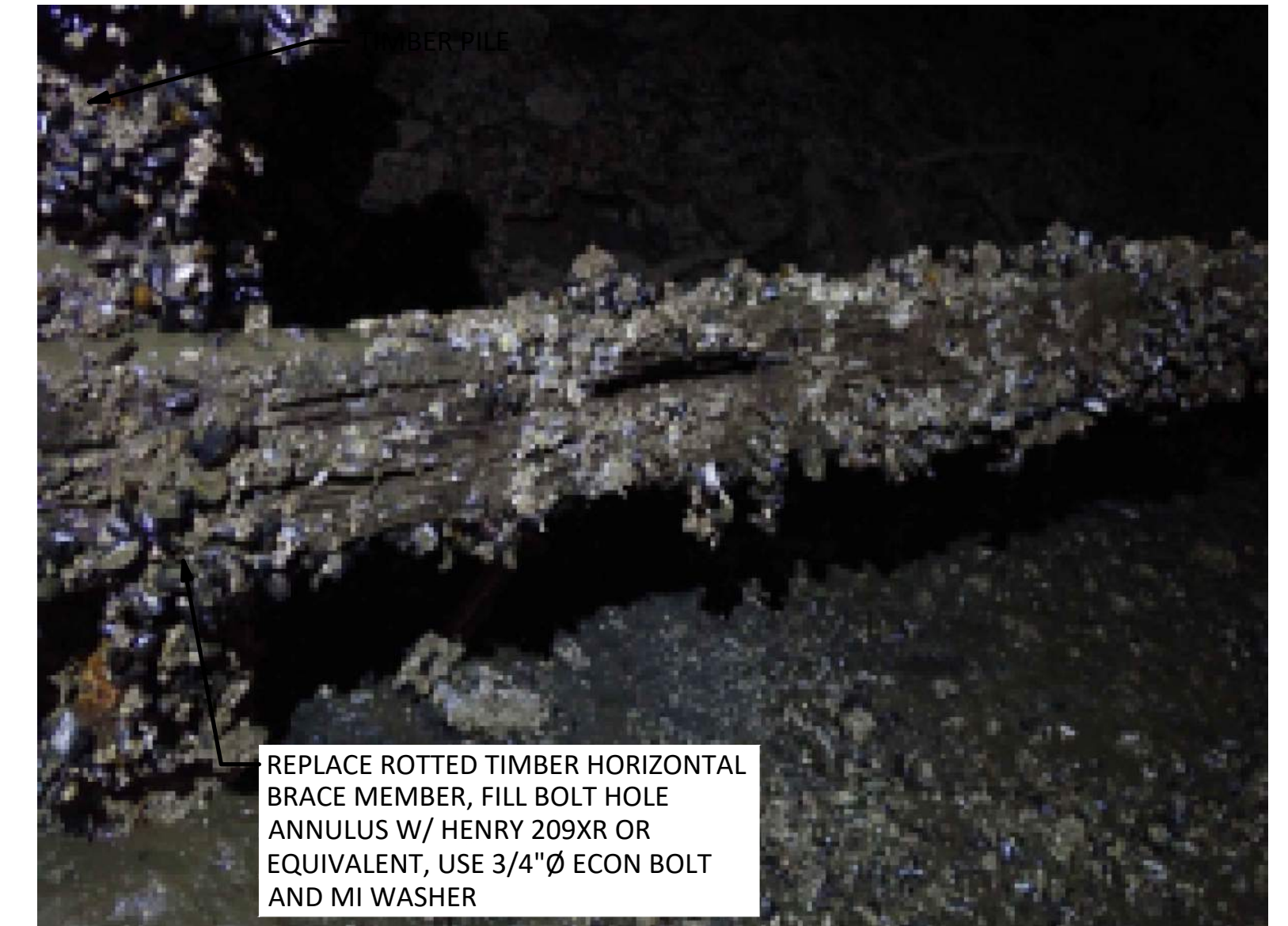
1. SEE GENERAL NOTES ON G-2 AND G-3.
2. DRAWING SHALL BE PRINTED IN COLOR.



7
S-4, S-7
PHOTO - 7
SCALE: NTS



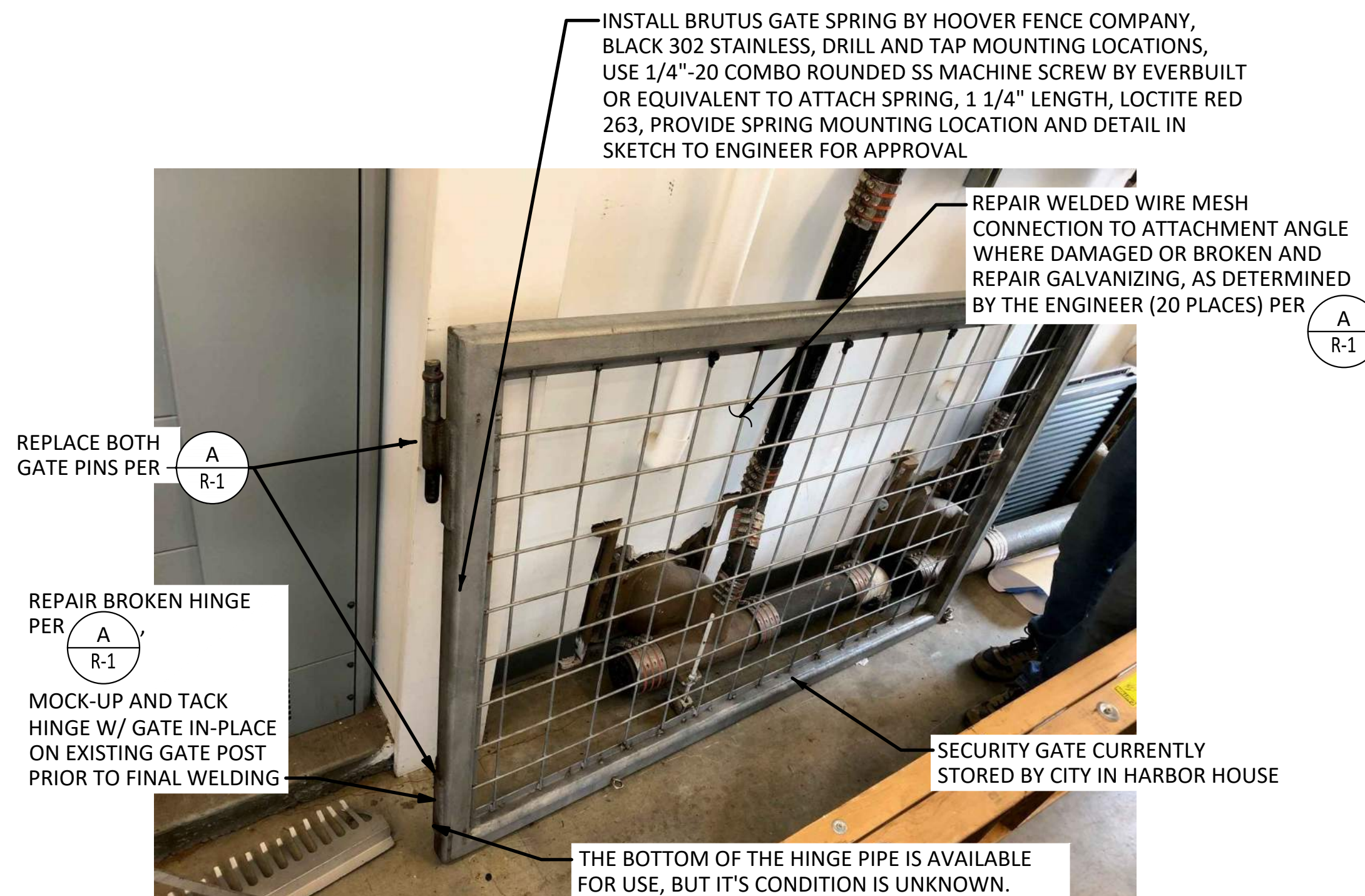
8
S-5
PHOTO - 8
SCALE: NTS



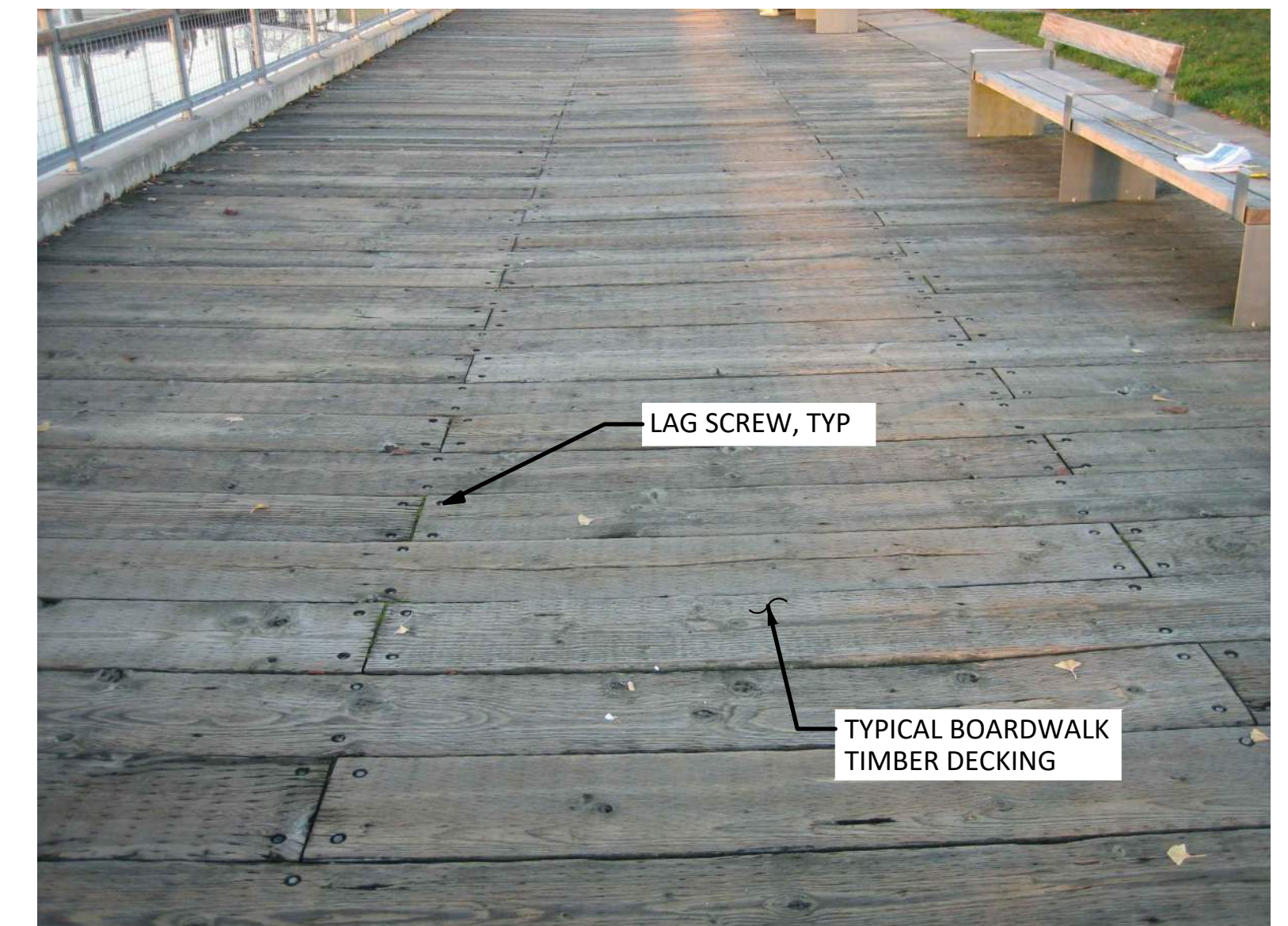
9
S-5
PHOTO - 9
SCALE: NTS



10
S-5, S-7, S-8, S-9
PHOTO - 10
SCALE: NTS



11
S-6
PHOTO - 11
SCALE: NTS - GATE SHALL SWING EASILY W/ OUT BINDING OR NOTICEABLE BINDING FRICTION. GATE SHALL CONTACT AND CLOSE AGAINST OPPOSITE POST SQUARELY. ALTHOUGH GATE WAS INSTALLED AT SOME POINT, CONTRACTOR SHALL PLAN TO REALIGN THE LOWER HINGE FOR PROPER SWING AND SQUARE IN CLOSED AND OPEN POSITIONS.



12
S-6, S-7
PHOTO - 12
SCALE: NTS

ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

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PROFESSIONAL ENGINEER
7/14/24

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APPROVED BY: JRG
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DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PHOTOS - SHEET 2

S-12
SHEET NO. **15** OF **21**

FOR BID

NOTES:

1. SEE GENERAL NOTES ON G-2 AND G-3.
2. DRAWING SHALL BE PRINTED IN COLOR.



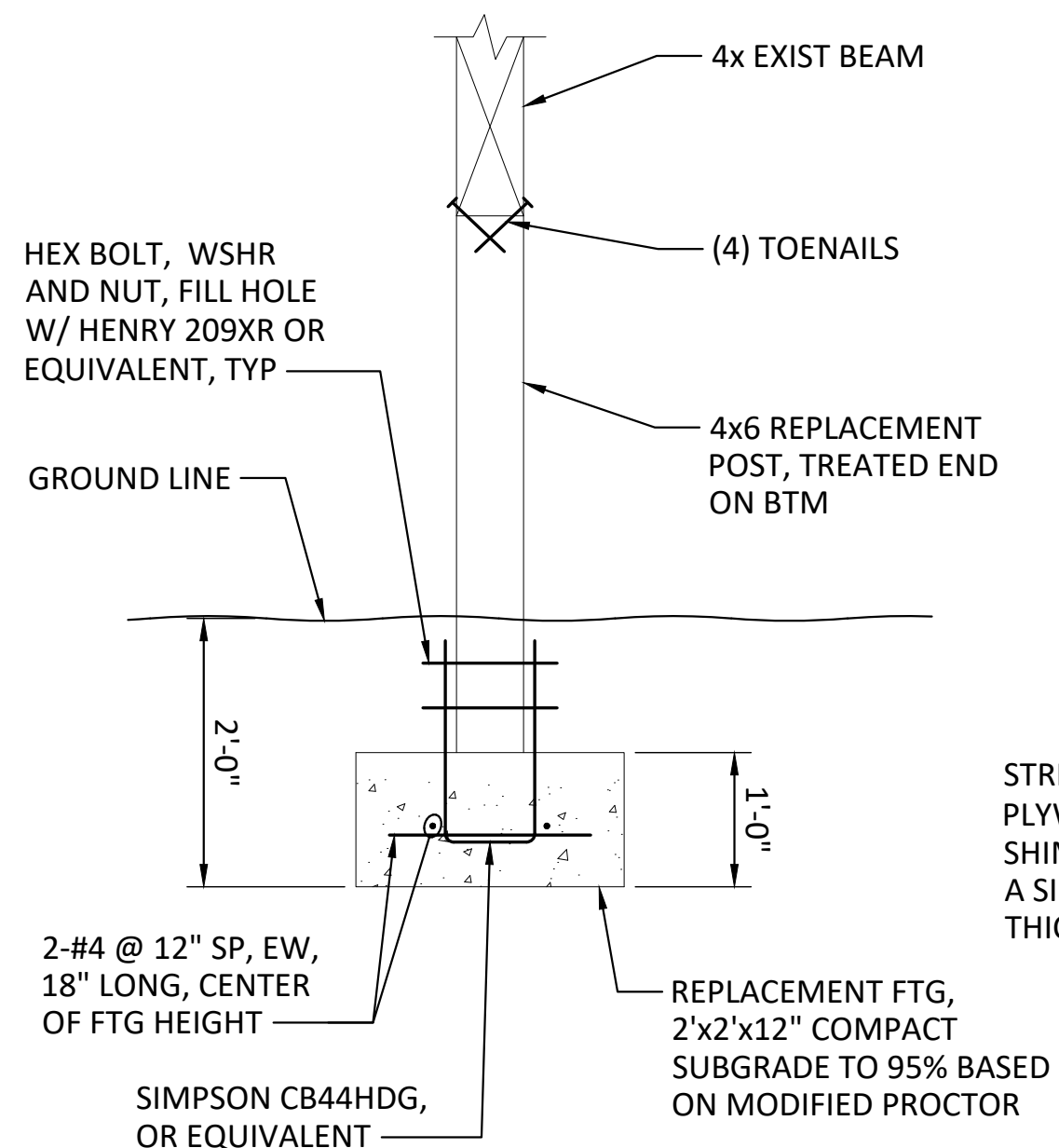
13
S-7 **PHOTO - 13**
SCALE: NTS



14
S-7 **PHOTO - 14**
SCALE: NTS



15
S-8 **PHOTO - 15**
SCALE: NTS



16
S-8 **PHOTO - 16 AND REPAIR DETAIL**
SCALE: NTS



17
S-8 **PHOTO - 17**
SCALE: NTS



18
S-9 **PHOTO - 18**
SCALE: NTS

ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

REVISIONS				
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 APPROVED BY: JRG
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**PERCIVAL LANDING
2021 REPAIRS**

REPAIR PHOTOS - SHEET 3

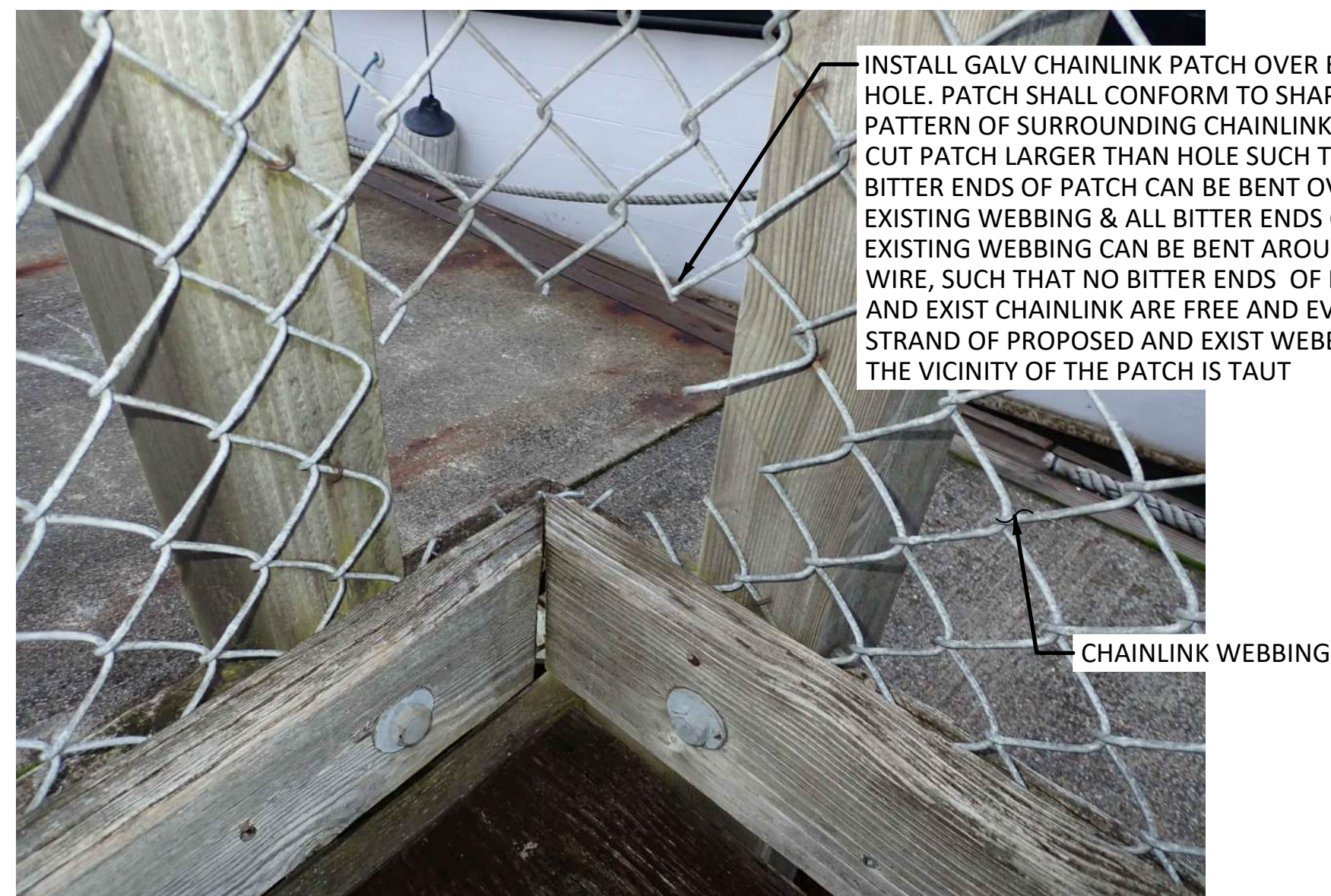
FOR BID

S-13

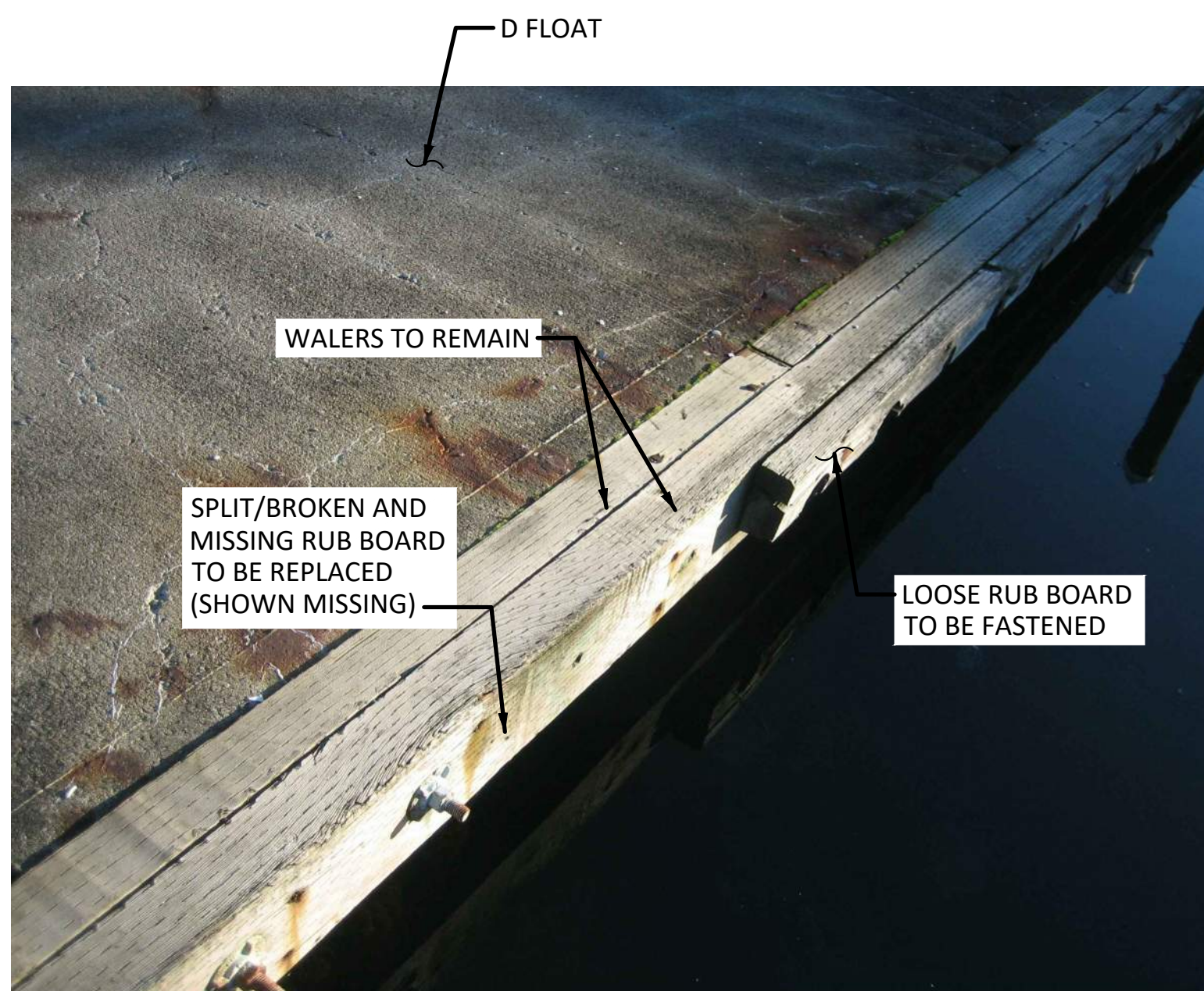
SHEET NO. **16** OF **21**

NOTES:

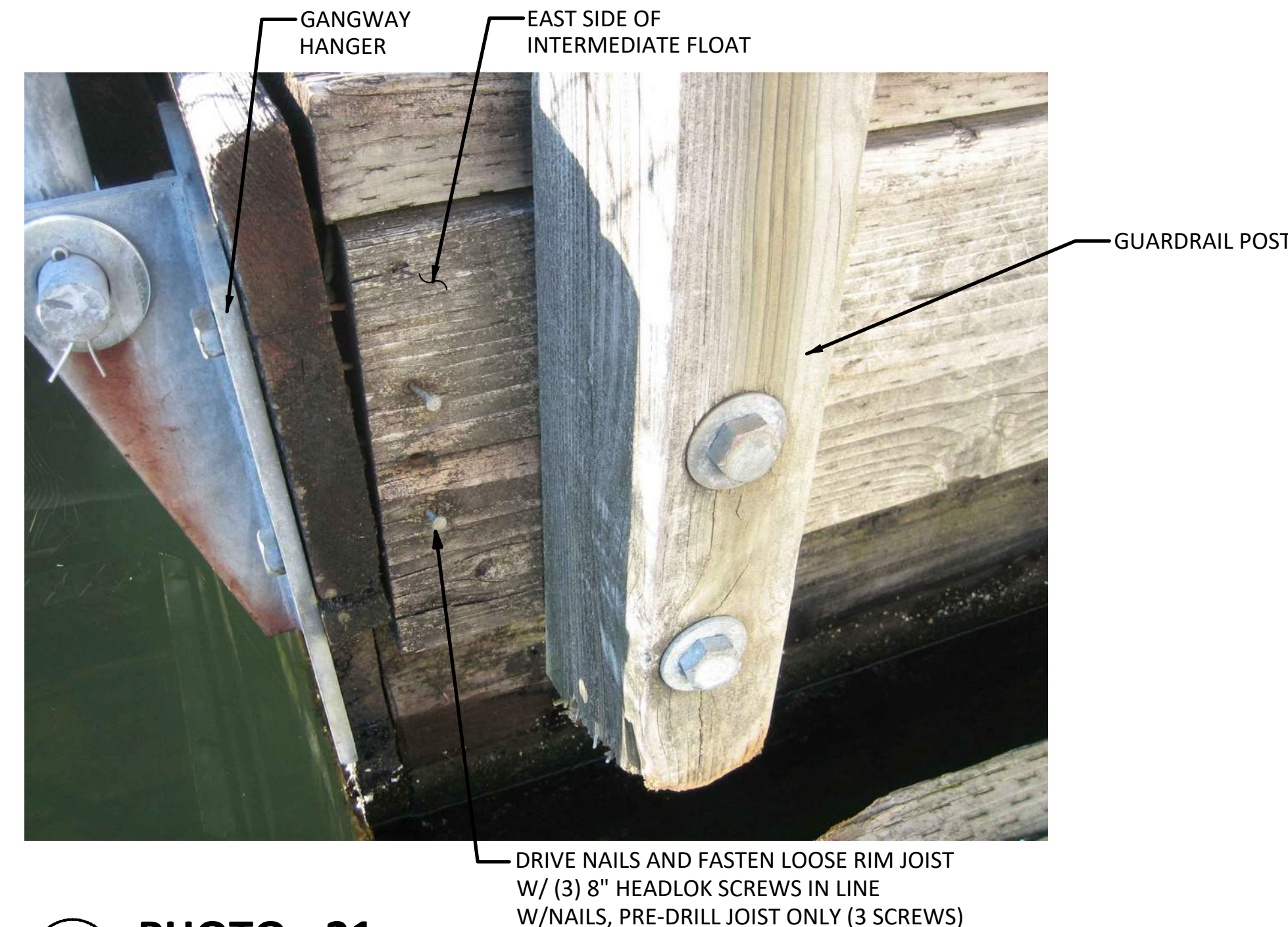
1. SEE GENERAL NOTES ON G-2 AND G-3.
2. DRAWING SHALL BE PRINTED IN COLOR.



19
S-10 **PHOTO - 19**
SCALE: NTS



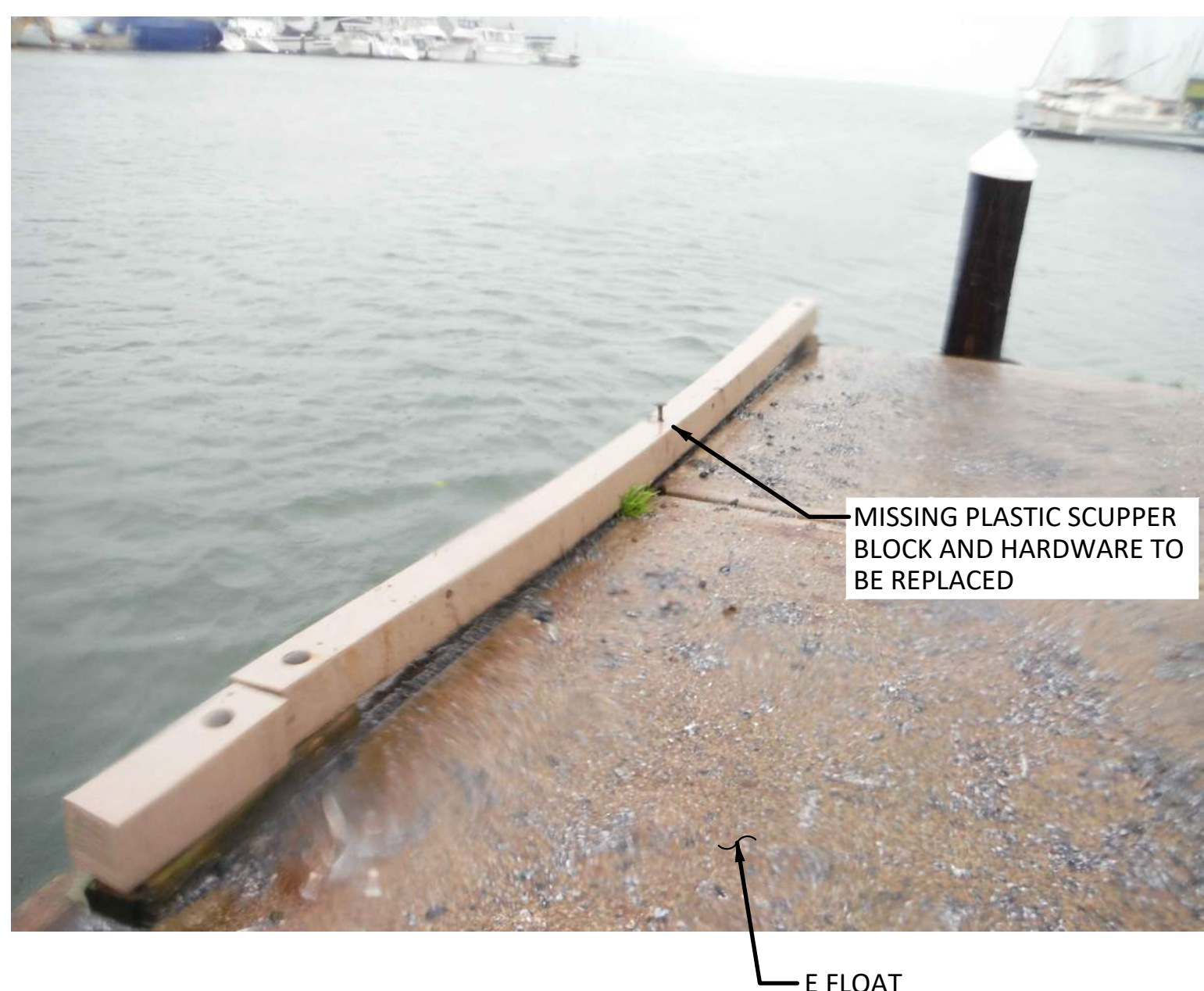
20
S-10 **PHOTO - 20**
SCALE: NTS



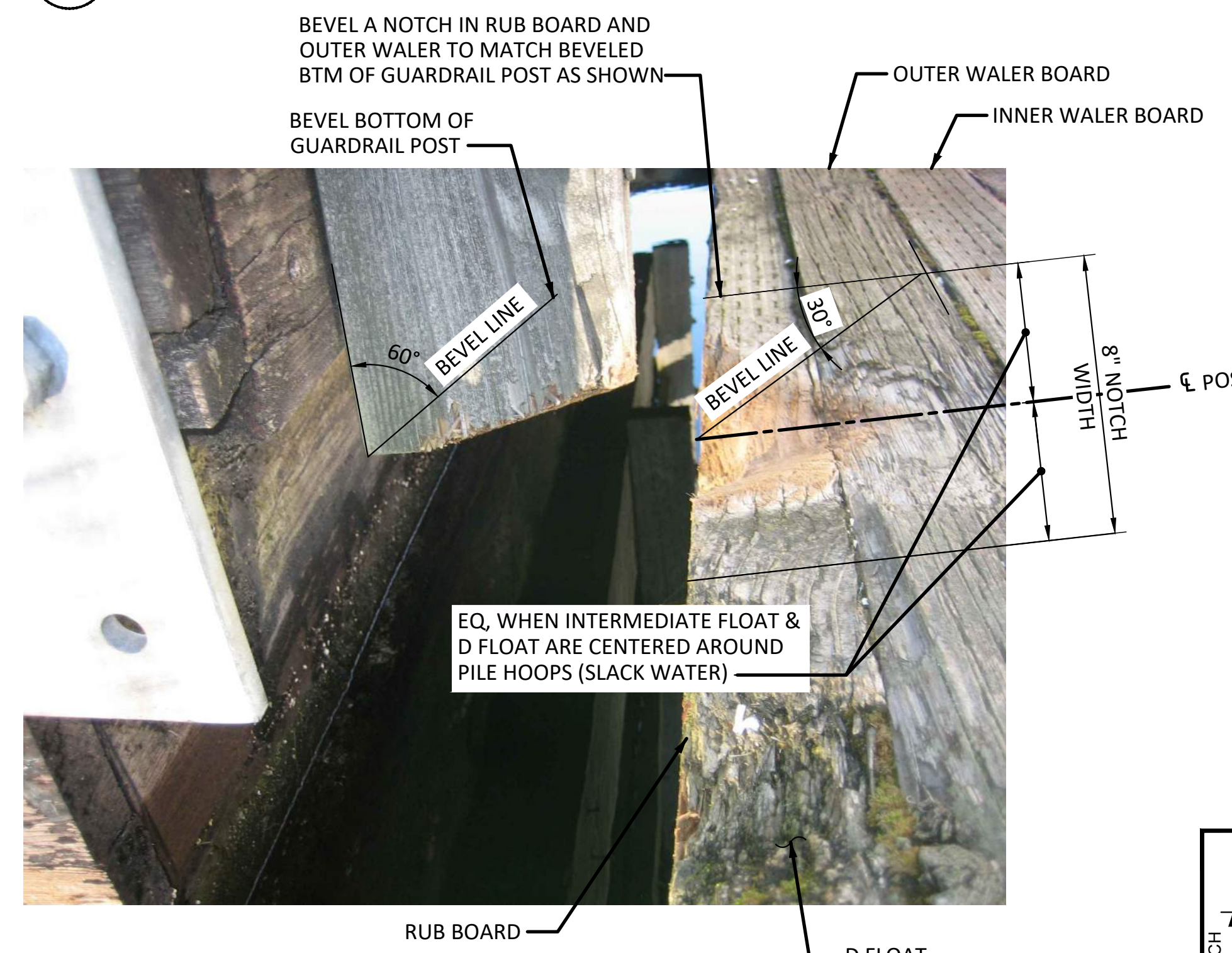
21
S-10 **PHOTO - 21**
SCALE: NTS



22
S-10 **PHOTO - 22**
SCALE: NTS



23
S-10 **PHOTO - 23**
SCALE: NTS



24
S-10 **PHOTO - 24**
SCALE: NTS

REVISIONS				
REV	DATE	BY	APPD	DESCRIPTION

DESIGNED BY: TWF
DRAWN BY: AHK
CHECKED BY: JRG
APPROVED BY: JRG
SCALE: AS SHOWN
DATE: September 14, 2021

**PERCIVAL LANDING
2021 REPAIRS**

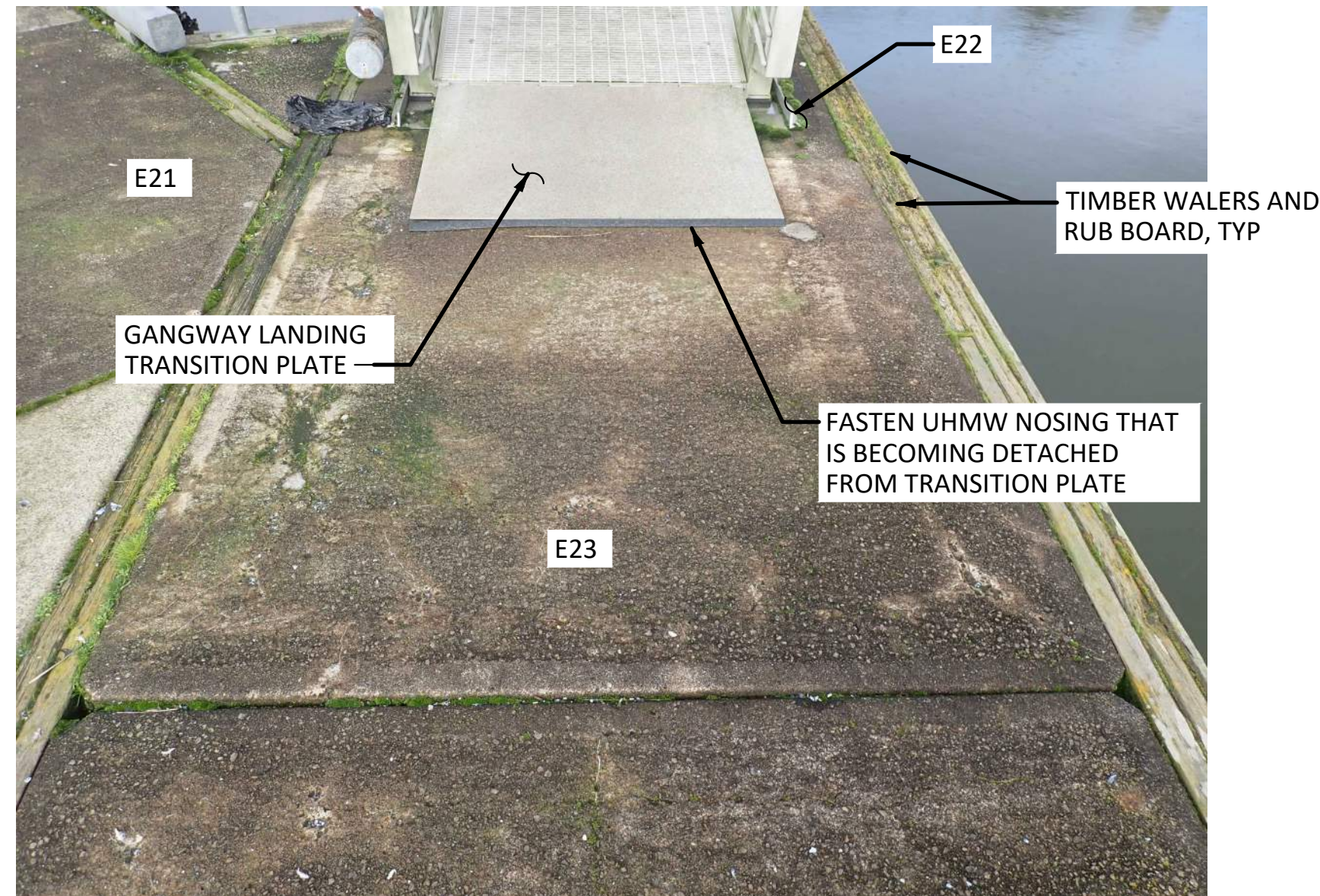
REPAIR PHOTOS - SHEET 4

FOR BID
S-14
SHEET NO. 17 OF 21

ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

NOTES:

1. SEE GENERAL NOTES ON G-2 AND G-3.
2. DRAWING SHALL BE PRINTED IN COLOR.



25
5-10 **PHOTO - 25**
SCALE: NTS

- REMOVE AND RE-INSTALL TRANSITION PLATE UHMW NOSING SS SCREWS THAT HAVE PULLED THROUGH UHMW NOSING. FOR SS SCREWS THAT CANNOT BE REMOVED, OR FOR EXISTING HOLES THROUGH UHMW THAT HAVE BEEN STRETCHED TO A POINT THAT SS SCREW HEADS DON'T PROPERLY BEAR, NEW SCREWS SHALL BE INSTALLED, WITHIN 1.5" OF THE EXISTING HOLE, ALONG SAME LINE WITH ALL OTHER HOLES. COUNTER SINK UHMW SIMILAR TO EXISTING FOR NEW SCREWS.






26
5-10 **PHOTO - 26**
SCALE: NTS

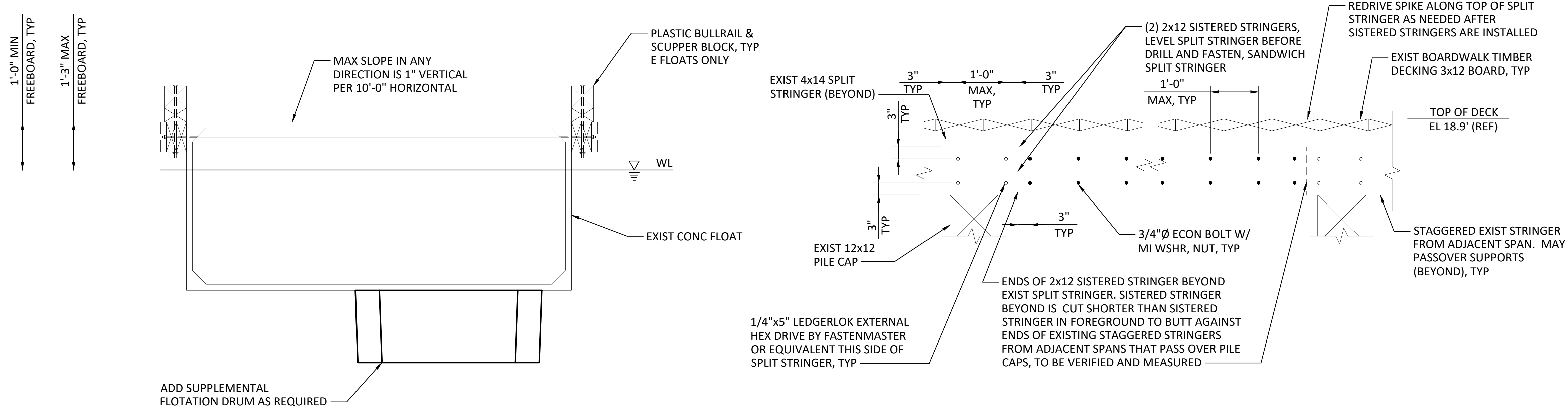
NOTES:

1. SUPPLEMENTAL FLOTATION WORK SHALL BE COMPLETE BEFORE INSTALLING GROUT PAD.
2. GROUT PAD SHALL BE SIKAQWICK® EZ PATCH OR EQUIVALENT.
3. GROUT SHALL BE NEAT.
4. MANUFACTURER RECOMMENDATIONS SHALL BE STRICTLY FOLLOWED, PLUS THE REQUIREMENTS OF THIS CONTRACT.
5. CONCRETE FLOAT SURFACE TO RECEIVE GROUT SHALL BE THOROUGHLY CLEANED W/ A STEEL WIRE CUP BRUSH DRIVEN BY ANGLE GRINDER. CONCRETE FLOAT UNIT E22 SURFACE TO RECEIVE GROUT SHALL BE SCARIFIED TO MEET GROUT SUPPLIER RECOMMENDATIONS. ANY CONCRETE DUST OR DEBRIS CREATED DURING SURFACE SCARIFICATION SHALL BE CAPTURED WITH APPROPRIATE MEANS SUCH AS DUST COLLECTION, TENTING, WET SUCTION, CONTAINMENT BARRIER, ETC., AND DISPOSED OF IN ACCORDANCE WITH ALL APPLICABLE LAWS, REGULATION AND STANDARDS.
6. CONCRETE SURFACE TO RECEIVE GROUT SHALL BE THOROUGHLY WASHED WITH 3,000 PSI POTABLE WATER RIGHT BEFORE APPLICATION OF GROUT TO REMOVE ALL RESIDUE, DEBRIS, DIRT, ORGANIC MATTER THAT COULD COMPROMISE THE GROUT BOND.
7. GROUT PAD SURFACE SHALL HAVE A BROOM FINISH.
8. ENGINEER SHALL BE GIVEN 2 DAYS NOTICE PRIOR TO PLACING GROUT PAD.

ONE INCH
AT FULL SIZE. IF NOT ONE
INCH SCALE ACCORDINGLY

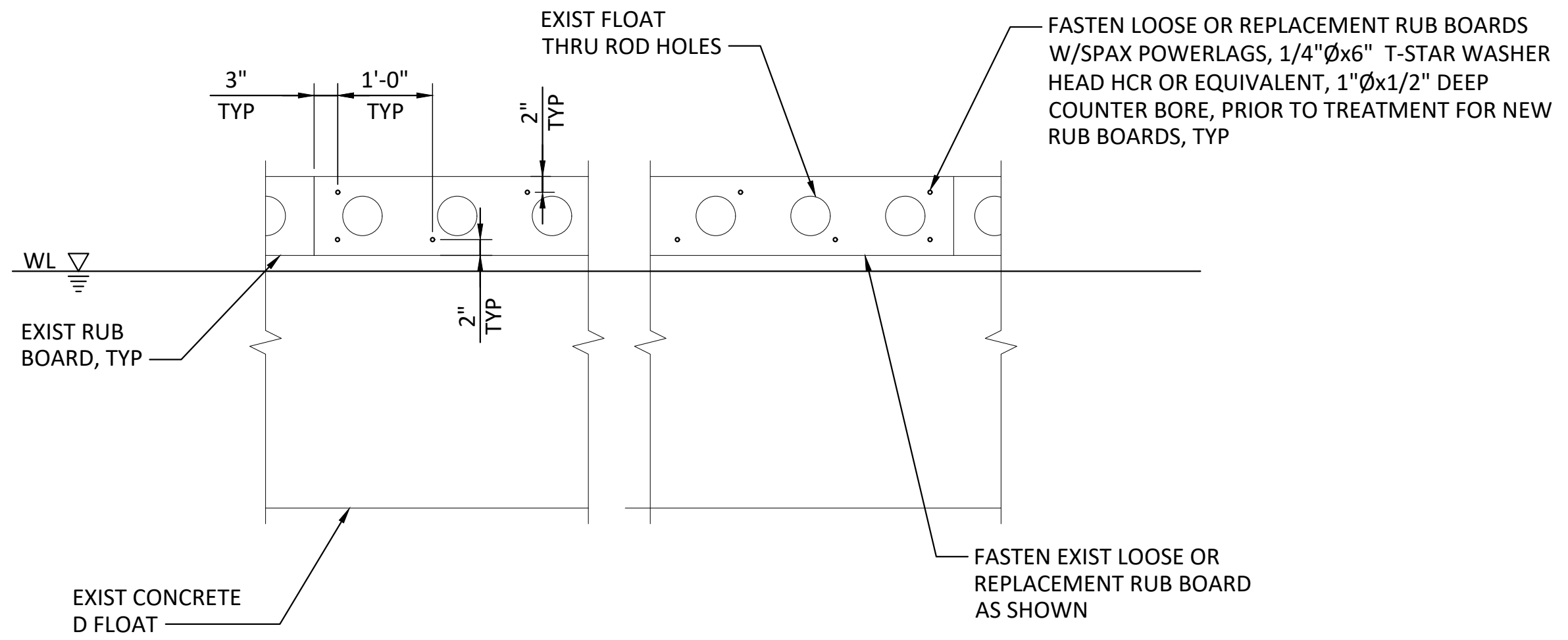
FOR BID

 <p>WSP USA Inc. 33301 9th Avenue South Suite 300 Federal Way, WA 98003-2600 TEL: (206) 431-2300 FAX: (206) 431-2250</p>			<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>REV</th> <th>DATE</th> <th>BY</th> <th>APPD</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>		REV	DATE	BY	APPD	DESCRIPTION																										<p>DESIGNED BY: TWF DRAWN BY: AHK CHECKED BY: JRG APPROVED BY: JRG SCALE: AS SHOWN DATE: September 14, 2021</p>	<p>PERCIVAL LANDING 2021 REPAIRS</p>	<p>S-15</p>
			REV	DATE	BY	APPD	DESCRIPTION																														
<p>REPAIR PHOTOS - SHEET 5</p>			<p>SHEET NO. 18 OF 21</p>																																		



1 **DETAIL - ADDITIONAL FLOTATION**
S-10 SCALE: 3/4" = 1'-0"

2 **DETAIL - ELEVATION SPLIT STRINGER REPAIR**
S-9 SCALE: 3/4" = 1'-0"



3 **DETAIL - D FLOAT RUB BOARD FASTENING**
S-10 SCALE: 3/4" = 1'-0"

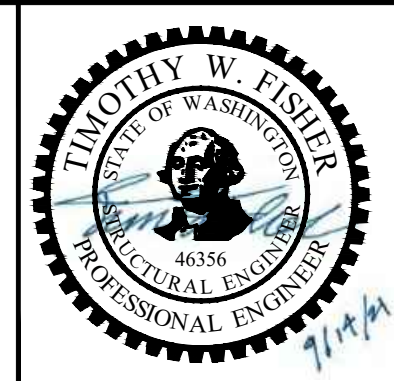
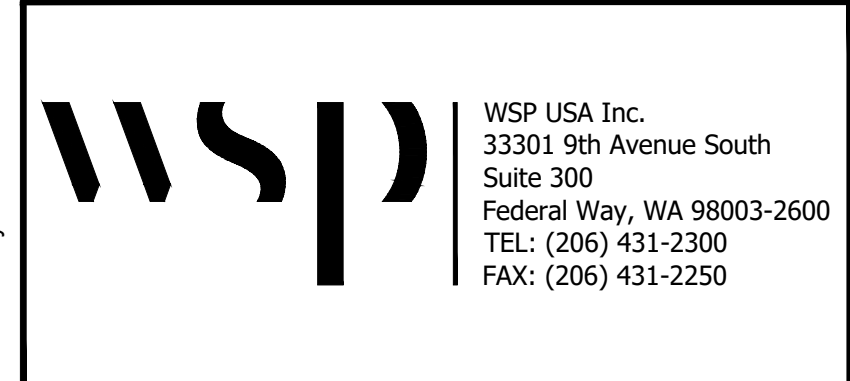
NOTES:

- SEE GENERAL NOTES ON G-2 AND G-3.
- DRUM LOCATIONS AND DRUM QUANTITY ARE ESTIMATED. CONTRACTOR SHALL DETERMINE THE LOCATION, SIZE AND QUANTITY OF DRUMS TO MEET THE FREEBOARD AND SLOPE REQUIREMENTS. 7 ADDITIONAL DRUMS SHALL BE ASSUMED FOR BIDDING PURPOSES, BEYOND THOSE IN THE TABLE. UNUSED DRUMS SHALL BECOME PROPERTY OF THE CONTRACTOR.
- MINIMUM FREEBOARD FOR FLOAT UNITS SHALL BE 12 INCHES AFTER SUPPLEMENTAL FLOTATION HAS BEEN ADDED. USE FOAM FILLED ACE ROTO-MOLD FLOTATION DRUMS OR EQUIVALENT.
- EXISTING CONCRETE FLOATS SHALL FLOAT LEVEL UNDER DEAD LOAD ONLY AND BE FLUSH WITH ADJACENT FLOAT UNITS. MAXIMUM OUT OF LEVEL TOLERANCE FOR TRANSVERSE AND LONGITUDINAL SLOPE SHALL BE 1 INCH PER 10 FEET AFTER SUPPLEMENTAL FLOTATION HAS BEEN ADDED.
- SUPPLEMENTAL FLOTATION UNITS MAY HAVE BEEN ADDED BENEATH D AND E FLOATS DURING PAST PROJECTS AND THE LOCATION OF THESE UNITS ARE NOT KNOWN. CONTRACTOR SHALL DIVE TO DETERMINE THE SIZE AND LOCATION OF EXISTING SUPPLEMENTAL FLOTATION BENEATH D AND E FLOATS, PRIOR TO PROCURING PROPOSED DRUMS. CONTRACTOR SHALL PROVIDE AN AS-BUILT SKETCH OF SIZE AND LOCATION FOR ALL EXISTING SUPPLEMENTAL FLOTATION BENEATH D AND E FLOATS. CONTRACTOR SHALL NOT STACK SUPPLEMENTAL FLOTATION UNITS/DRUMS ON TOP OF EACH OTHER BENEATH D AND E FLOATS. CONTRACTOR MAY HAVE TO MOVE OR REMOVE (DEMOLISH) EXISTING SUPPLEMENTAL FLOTATION UNITS WHERE ADDITIONAL SUPPLEMENTAL FLOTATION IS NEEDED, TO ACHIEVE REQUIRED FREEBOARD.
- CONTRACTOR SHALL BASE FLOTATION DRUM MODEL SELECTION AND QUANTITY FOR PROCUREMENT ON FREEBOARD MEASUREMENTS THAT THE CONTRACTOR COLLECTS OF D AND E FLOATS AND CONTRACTOR'S BUOYANCY CALCULATIONS. CONTRACTOR SHALL SUBMIT BUOYANCY CALCULATIONS TO ENGINEER FOR APPROVAL.

ADDITIONAL FLOTATION DRUM ESTIMATED LOCATIONS	
D1	E10
D3	E11
D8	E12
D10	E13
D12	E22
D24	E34
D25	E35
E1	E36
E2	E37
E3	E38
E6	E57
E7	E76
E8	E77
E9	

ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY

FOR BID



REVISIONS				
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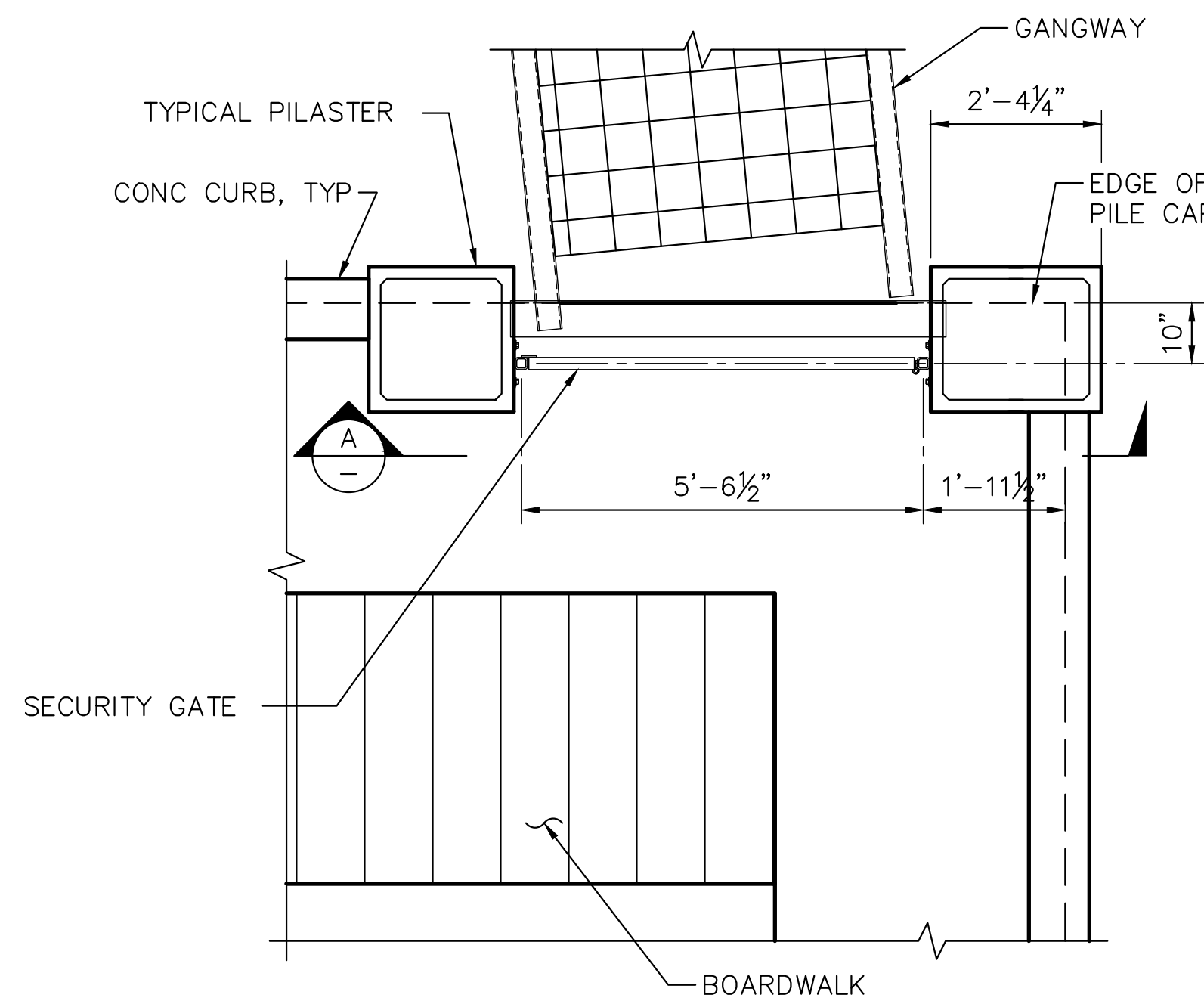
DESIGNED BY: TWF
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**PERCIVAL LANDING
2021 REPAIRS**

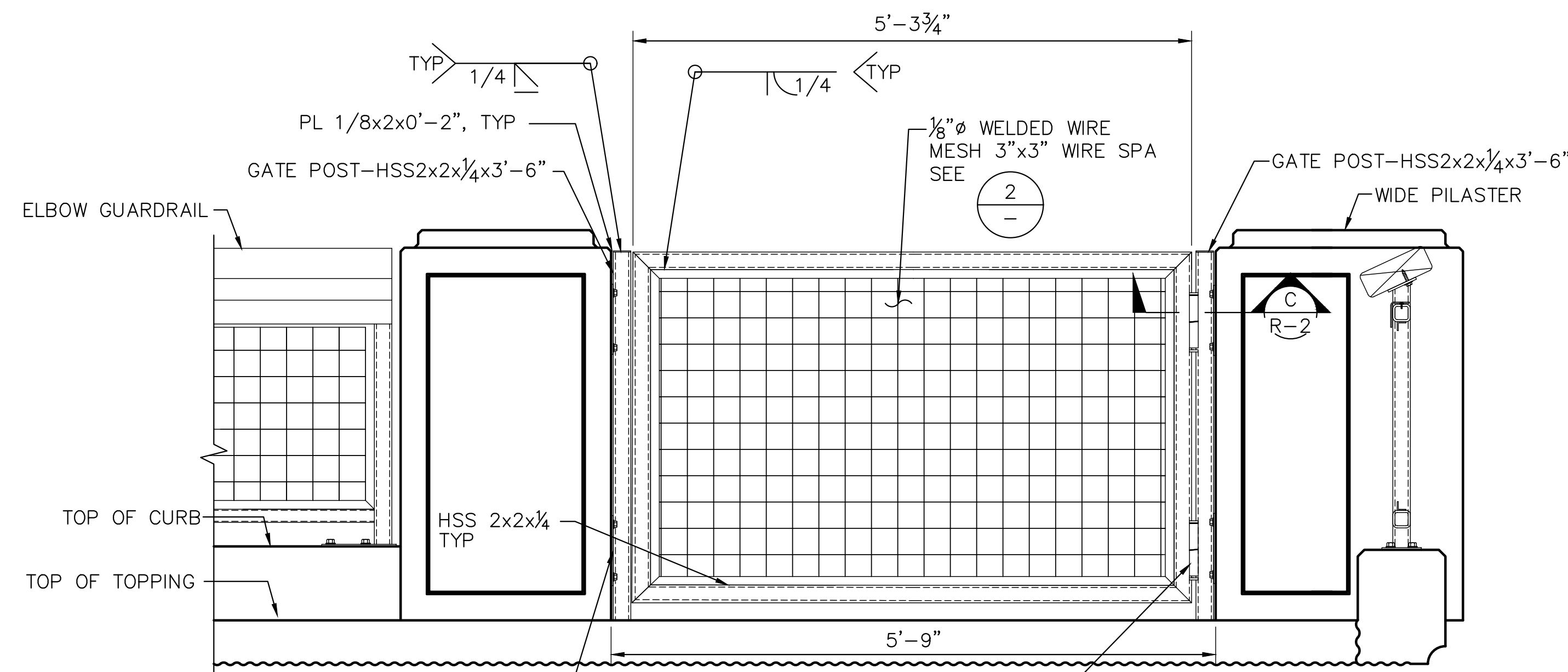
DETAILS

S-16

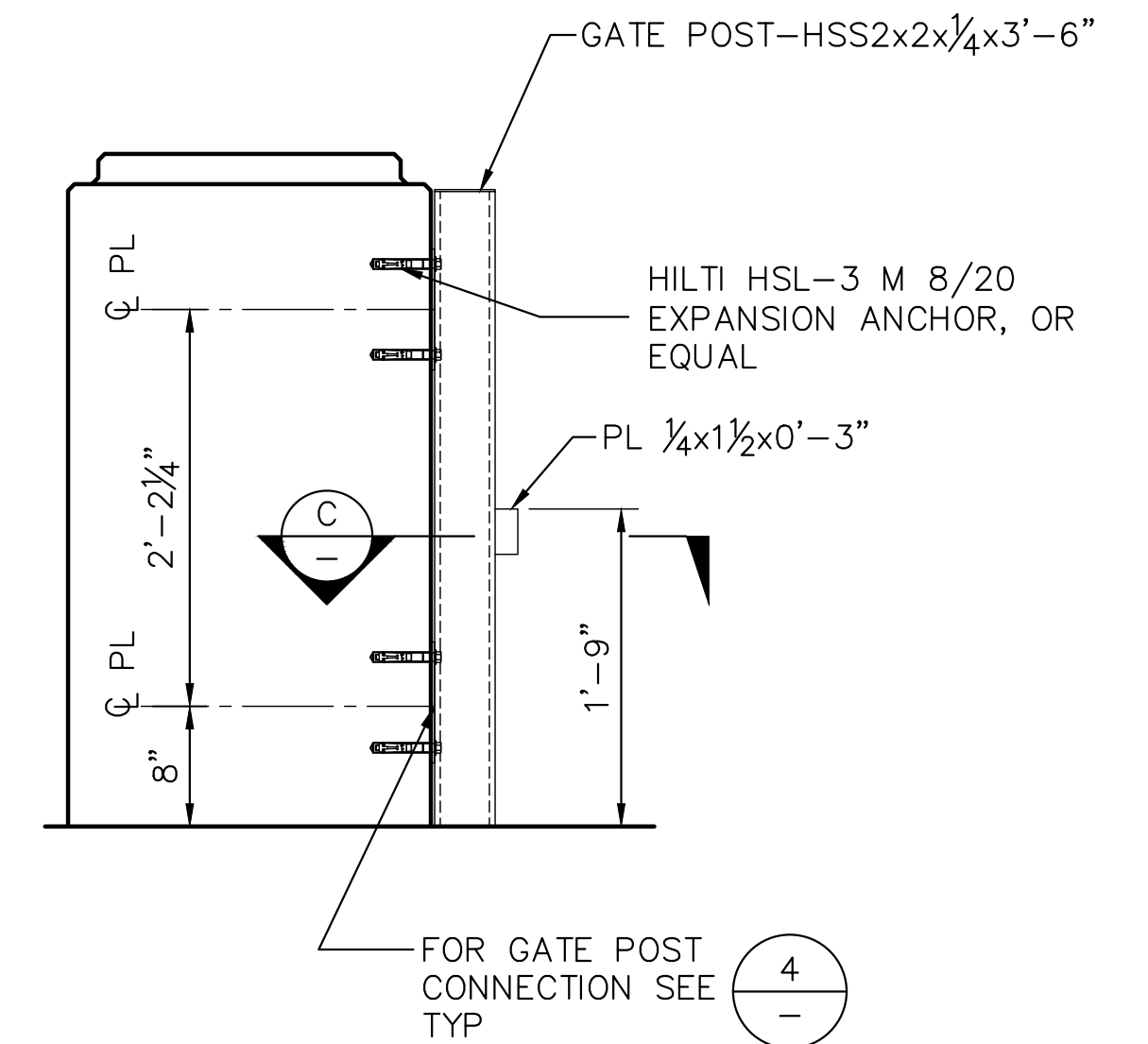
SHEET NO. **19** OF **21**



1 PLAN - SECURITY GATE
 SCALE: 1/2"=1'-0"
 scale feet

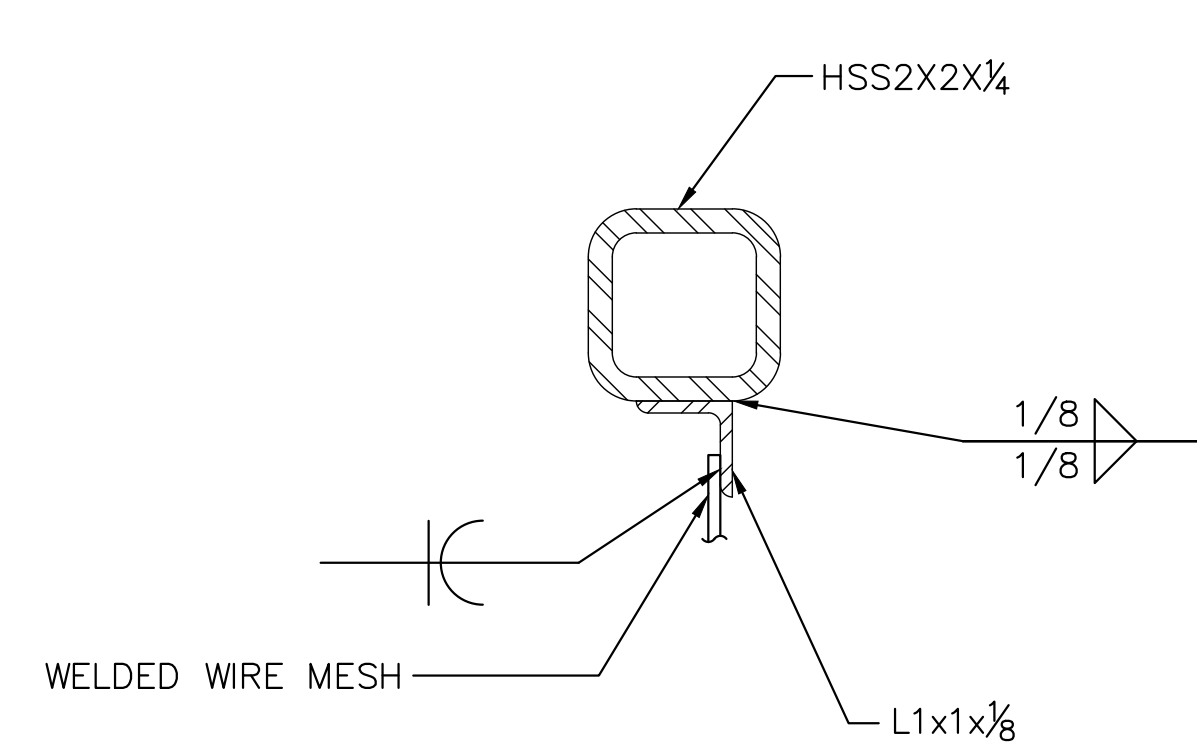


A ELEVATION - SECURITY GATE
 SCALE: 1"=1'-0"
 scale feet

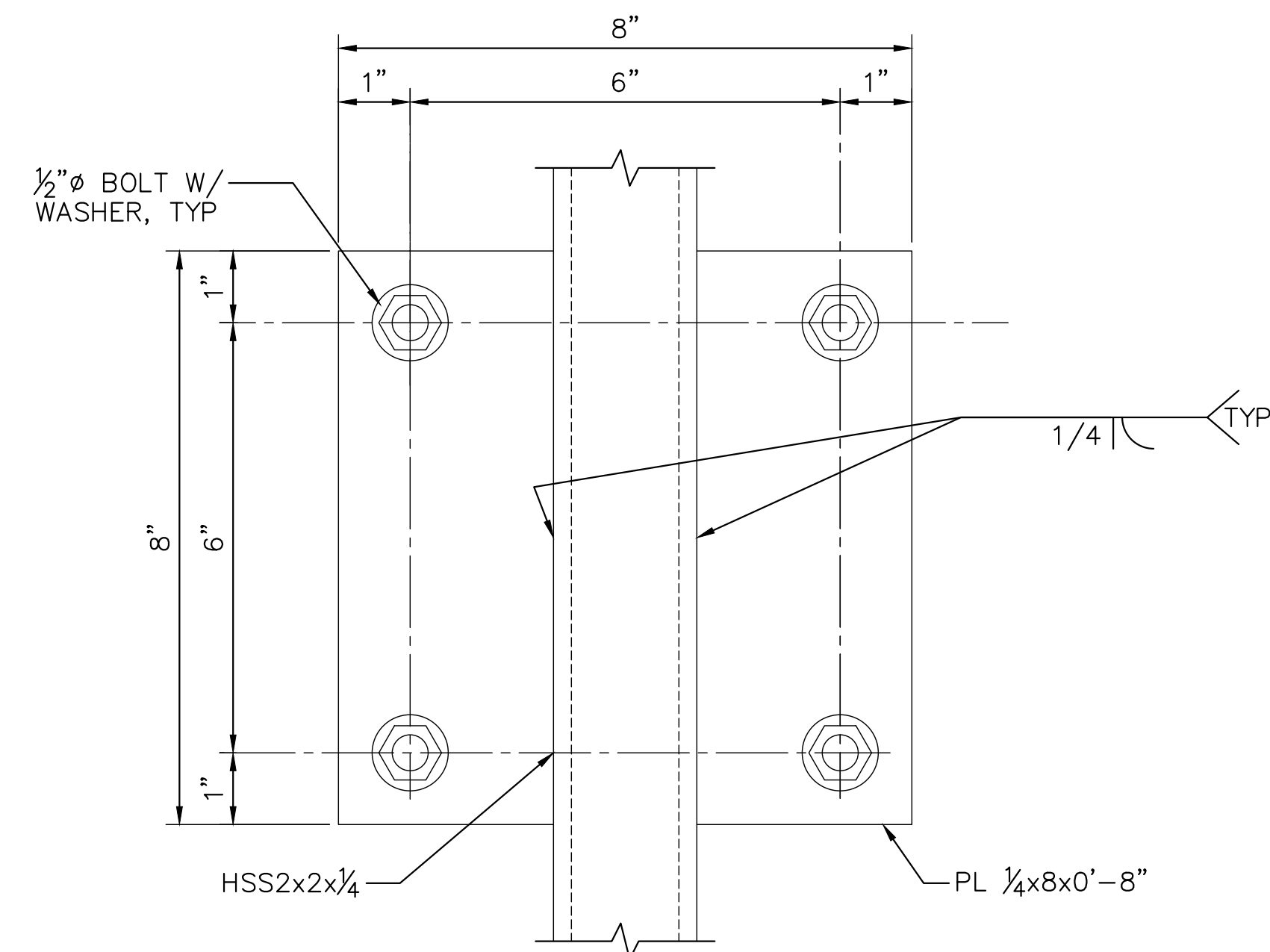


3 DETAIL - SECURITY GATE STOP
 SCALE: 1"=1'-0"
 scale feet

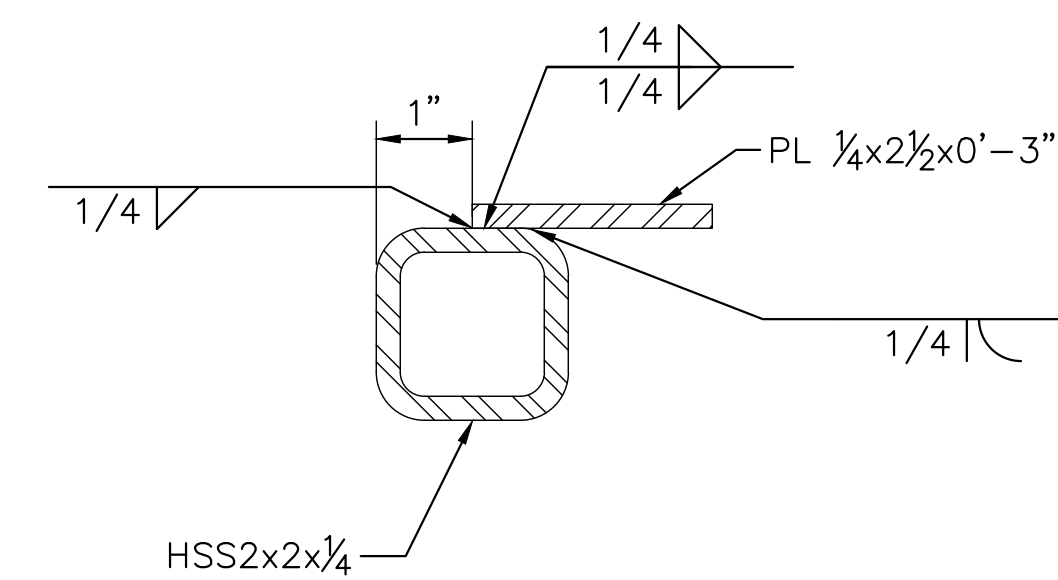
- NOTES:**
- HINGES SHALL BE ATTACHED TO THE GATE POST SUCH THAT THEY ARE CENTERED WITH THE GATE POST CONNECTION PLATES.
 - THE SPACE BETWEEN THE SECURITY GATE AND THE GATE POST OPPOSITE THE HINGES SHALL BE 1/4"



2 DETAIL - ANGLE ATTACHEMENT
 SCALE: 6"=1'-0"
 scale inches

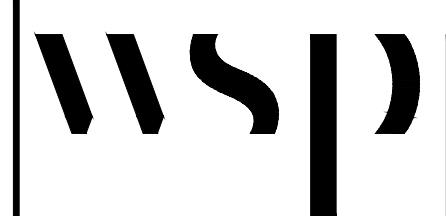


4 DETAIL - GATE POST CONNECTION PLATE
 SCALE: 6"=1'-0"
 scale inches



C SECTION - SECURITY GATE STOP
 SCALE: 6"=1'-0"
 scale inches

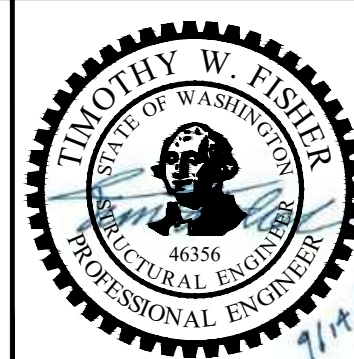
REFERENCE DRAWING



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 33301 9th Avenue South
 Suite 300
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 CHECKED BY: CSB
 APPROVED BY: RER
 SCALE: AS SHOWN
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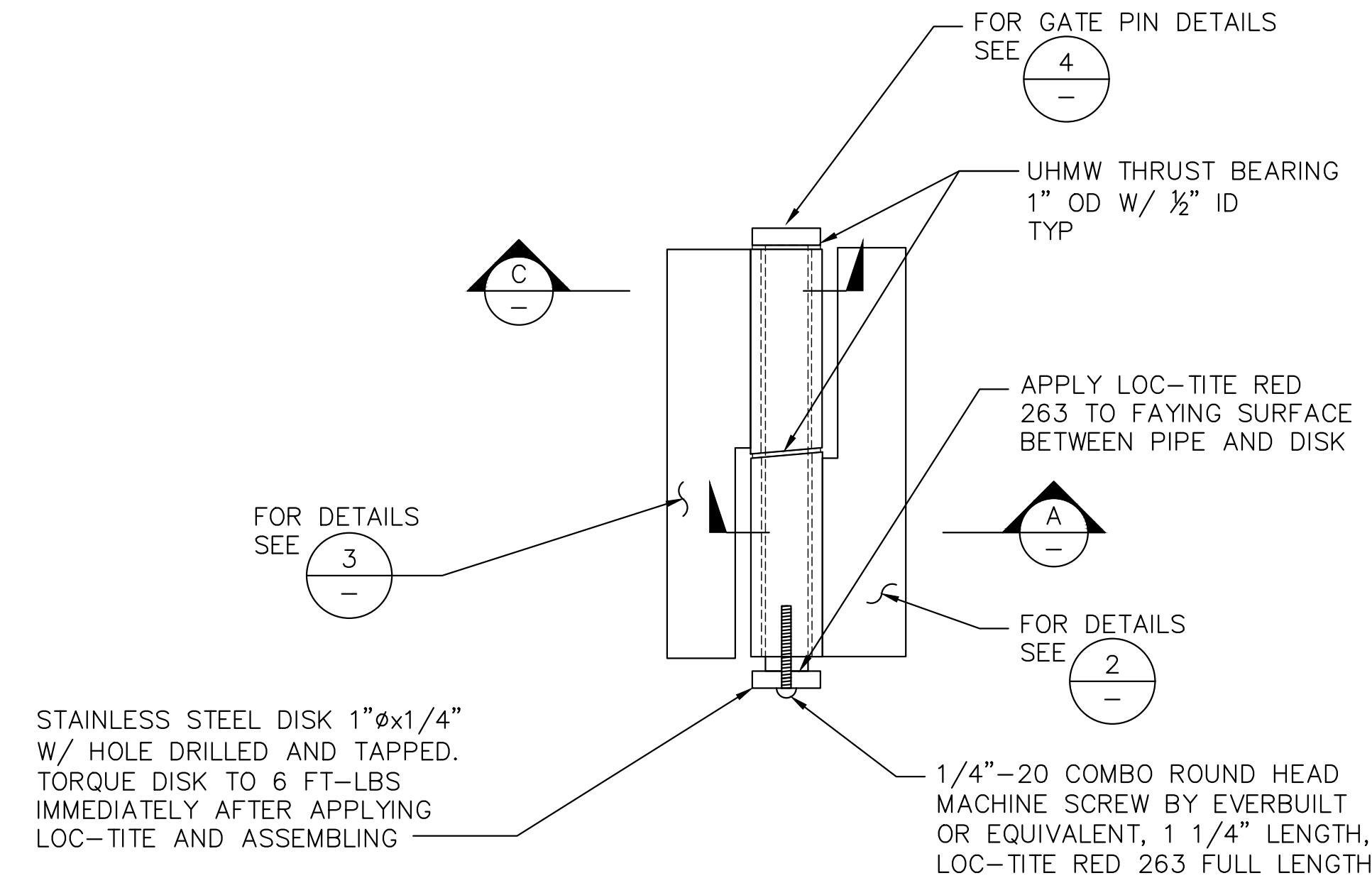
**PERCIVAL LANDING
 2021 REPAIRS**

BOARDWALK DETAILS - SHEET 1 (REFERENCE ONLY)

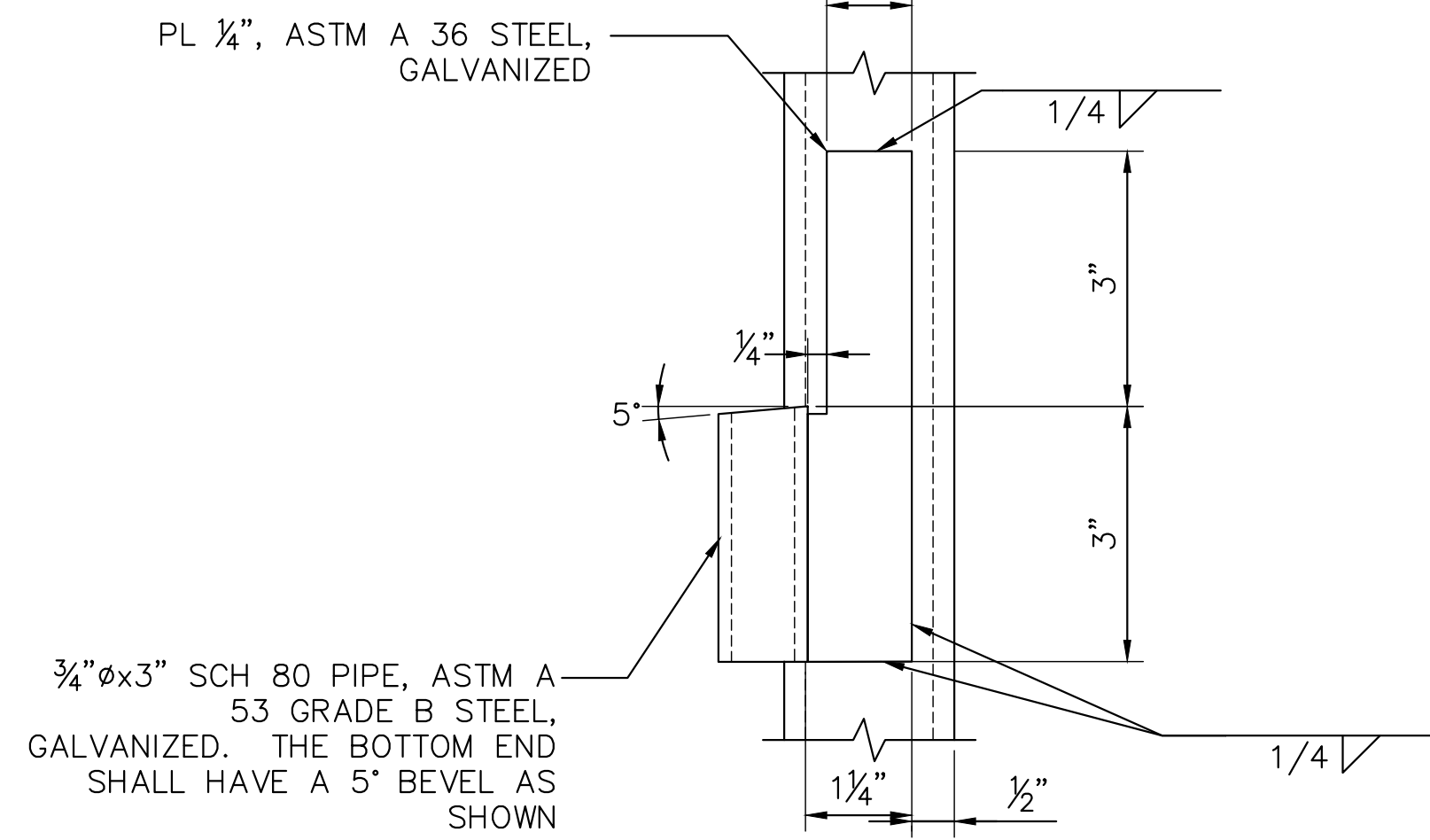
R-1

SHEET NO. 20 OF 21

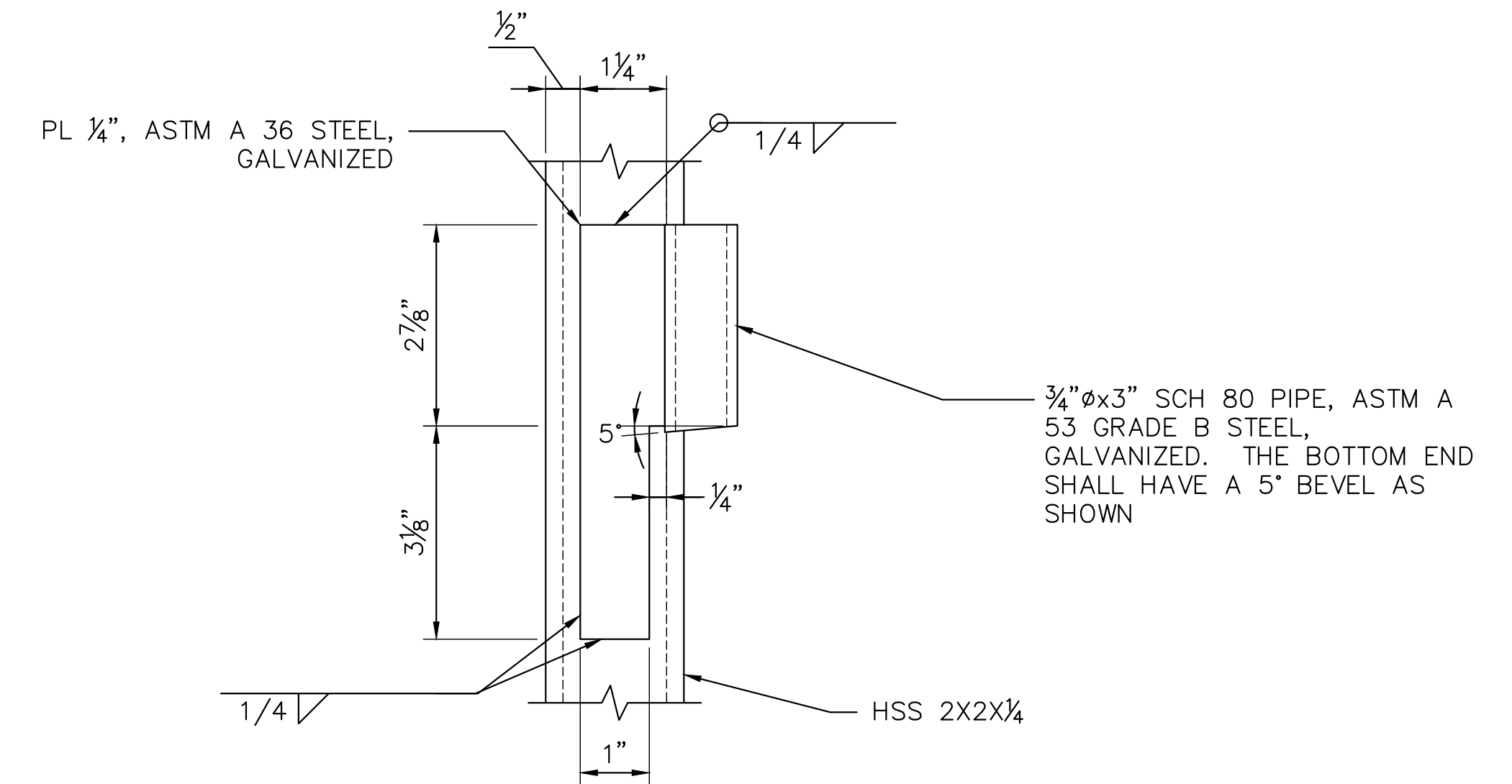
ONE INCH
 AT FULL SIZE, IF NOT ONE
 INCH SCALE ACCORDINGLY



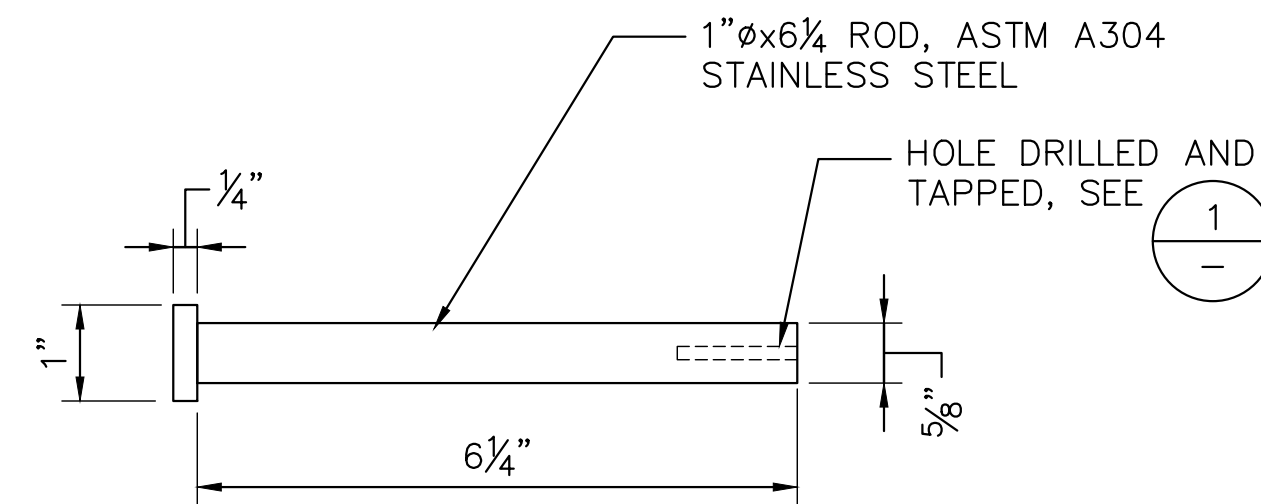
1
S-35
DETAIL - GATE HINGE
SCALE: 6"=1'-0"
0 2 4
scale inches



2
-
DETAIL - PLATE ATTACHED TO THE POST
SCALE: 6"=1'-0"
0 2 4
scale inches

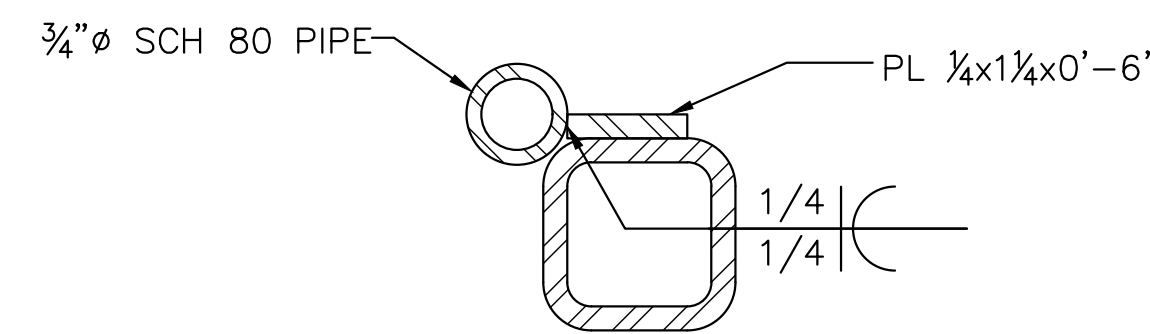


3
-
DETAIL - PLATE ATTACHED TO THE GATE
SCALE: 6"=1'-0"
0 2 4
scale inches

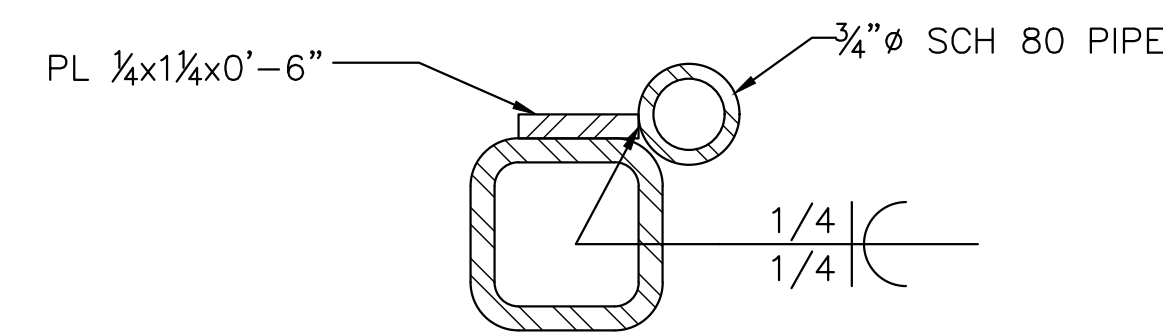


NOTE:
MACHINE THE 1"Ø ROD TO THE DIMENSIONS SHOWN

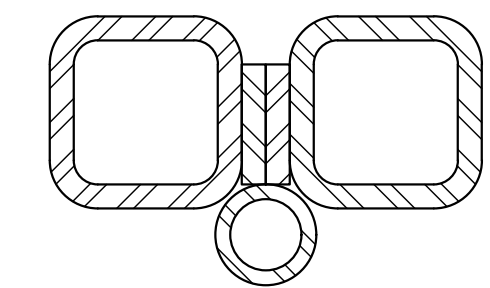
4
-
DETAIL - STAINLESS STEEL GATE PIN
SCALE: 6"=1'-0"
0 2 4
scale inches



A
-
SECTION - POST HINGE
SCALE: 6"=1'-0"
0 2 4
scale inches



C
R-1
SECTION - GATE HINGE
SCALE: 6"=1'-0"
0 2 4
scale inches



D
S-35
SECTION - GATE HINGE
SCALE: 6"=1'-0"
0 2 4
scale inches

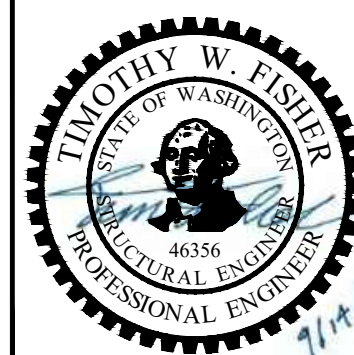
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**PERCIVAL LANDING
2021 REPAIRS**

BOARDWALK DETAILS - SHEET 2 (REFERENCE ONLY)

R-2

SHEET NO. **21** OF **21**

ONE INCH
AT FULL SIZE, IF NOT ONE
INCH SCALE ACCORDINGLY